

Appendix 2

HACKNEY WICK & FISH ISLAND

SUPPLEMENTARY PLANNING DOCUMENT (CONSULTATION DRAFT - XXXXXXXX 2016)



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1. INTRODUCTION

Hackney Wick and Fish Island are unique places within East London where the established but changing mainly employment focused areas to the west of the Lea Navigation/Hackney Cut face the Queen Elizabeth Olympic Park to its East, with its sporting venues and Here East with the emerging development of new homes at East Wick and Sweetwater. The challenge addressed in the adopted Local Plan (July 2015) is one of managing change to build on the historic character, providing a much needed supply of new homes and building on the developing and changing business base. The development of the new Neighbourhood Centre identified in the Local Plan is perhaps the lynchpin that will help tie together the existing and developing communities that make up this area.

The policies that underwrite this strategy are set out in the Local Plan (July 2015). This Supplementary Planning Document (SPD) has been prepared to provide further guidance to those policies and help communities, businesses and developers understand how those policies might be implemented. It also provides guidance, including design guidance, that will help planning proposals to reflect the historic and emerging character of the area in the way they are designed and relate to the buildings, streets and other spaces around them.

This is a consultation draft of the SPD and your views are sought on the content of the document. It should be noted that SPD's cannot make new policy. The policies are contained only within the Local Plan. These will only be reviewed when the Local Plan itself is reviewed. The SPD does, however, draw on much of the evidence and background information for Hackney Wick and Fish Island that was used to develop the Local Plan. Comments provided should be specifically about the contents of the SPD.

The consultation closes at 5pm on **DATE XXXX.**

Comments should be made in writing to:

**Hackney Wick Fish Island SPD
Consultation,
Planning Policy,
London Legacy Development
Corporation,
10th Floor, 1 Stratford Place, London.
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Email: planningpolicy@londonlegacy.co.uk

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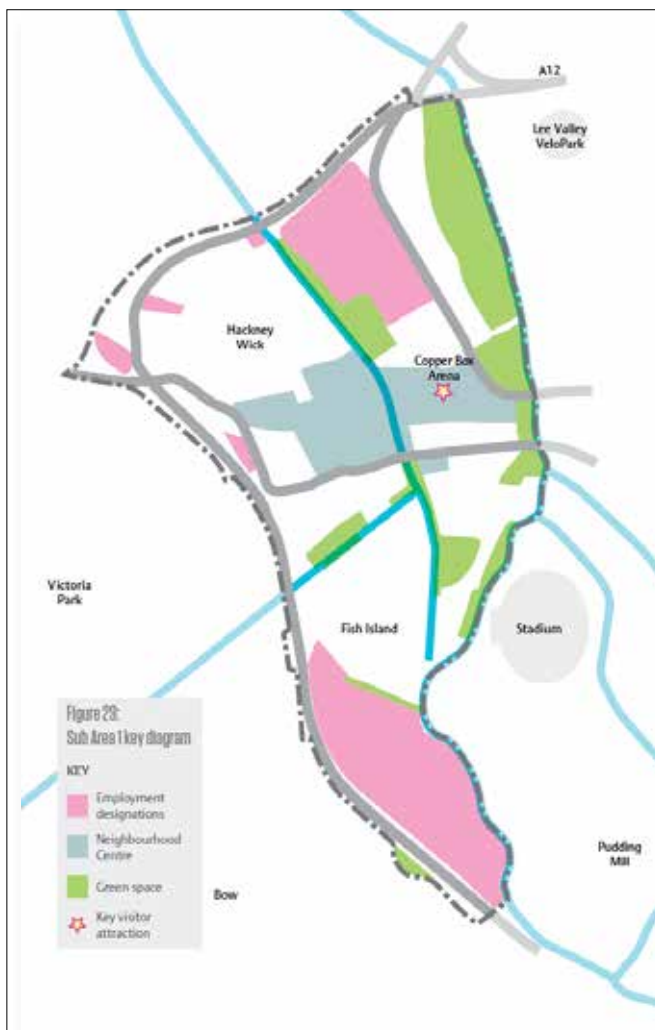


2. WHAT THE LOCAL PLAN SAYS

Vision

Hackney Wick and Fish Island will become a more vibrant, diverse and well connected series of mixed and balanced neighbourhoods with places of social, cultural and economic activity. The established residential areas in the north, historic character in the centre, and industrial areas to the south, will have been complemented by a mix of new homes, employment floorspace and community facilities around and within buildings of historic interest, a new Neighbourhood Centre and an upgraded railway station.

These will be served by and have direct access to the open spaces and world-class sporting facilities of Queen Elizabeth Olympic Park. A new digital quarter of hi-tech, media, broadcasting and education activities will be clustered within and around Here East, and complemented by a significant presence of creative and cultural industries producing bespoke and artistic products west of the Lee Navigation.



Area Priorities & Development Potential

- Heritage-led regeneration and high-quality design
- New Neighbourhood Centre
- Creative and productive employment
- A mix of uses
- Clusters of activity
- Connectivity
- Waterways and open spaces
- Communities and community facilities
- Flooding
- Strategic links
- Approximately 4,500 new homes
- At least 30,000 sqm of new employment (B use class) space
- Protection and Intensification of Strategic Industrial Land (SIL) areas
- Approximately 20,000 sqm of new retail, leisure and community floorspace (Use Classes A1-A5, and D1-D2).

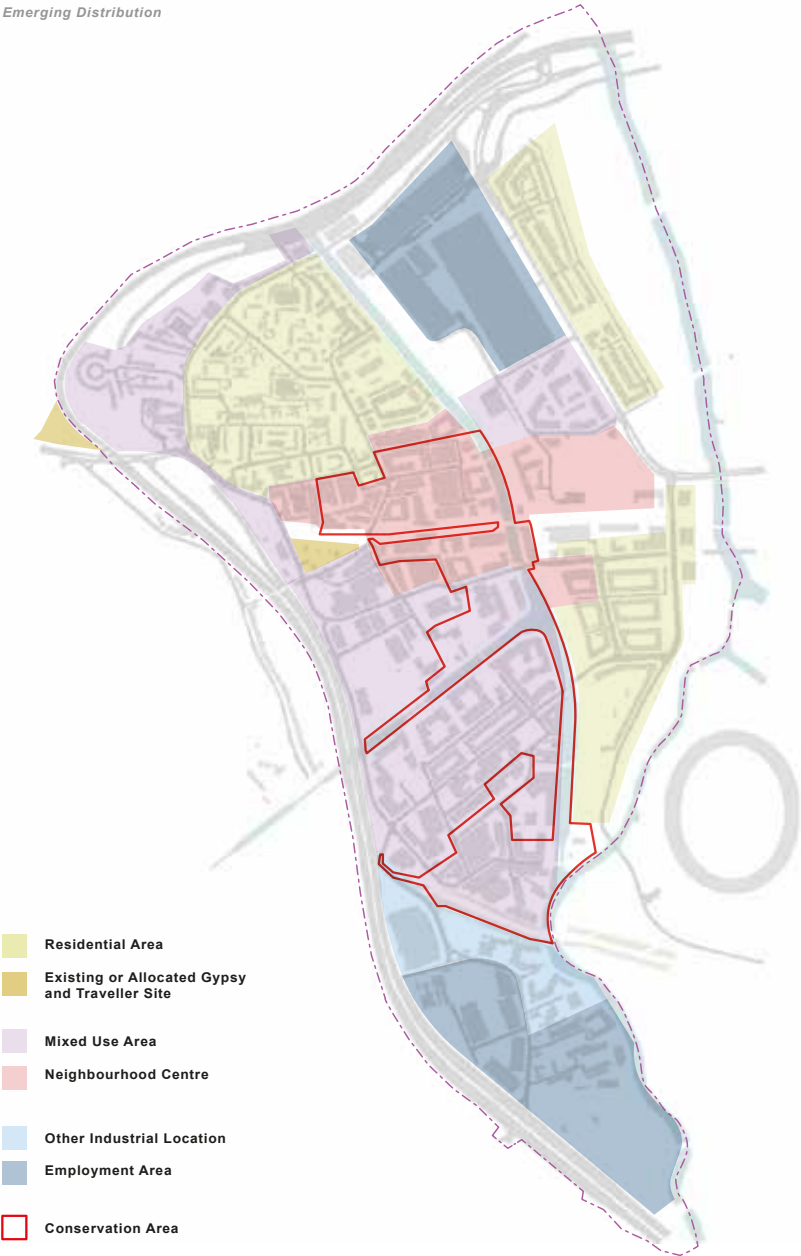
LAND-USE AND ALLOCATED SITES

Nine sites have been specifically allocated for new development within Hackney Wick and Fish Island, within the Local Plan. These are not repeated within this SPD and so specific reference to the site allocation policies for each within the Local Plan will be necessary.

The majority of policies in the Local Plan will be relevant to development proposals in Hackney Wick and Fish Island. Key policies are also highlighted within each guidance section of this document.

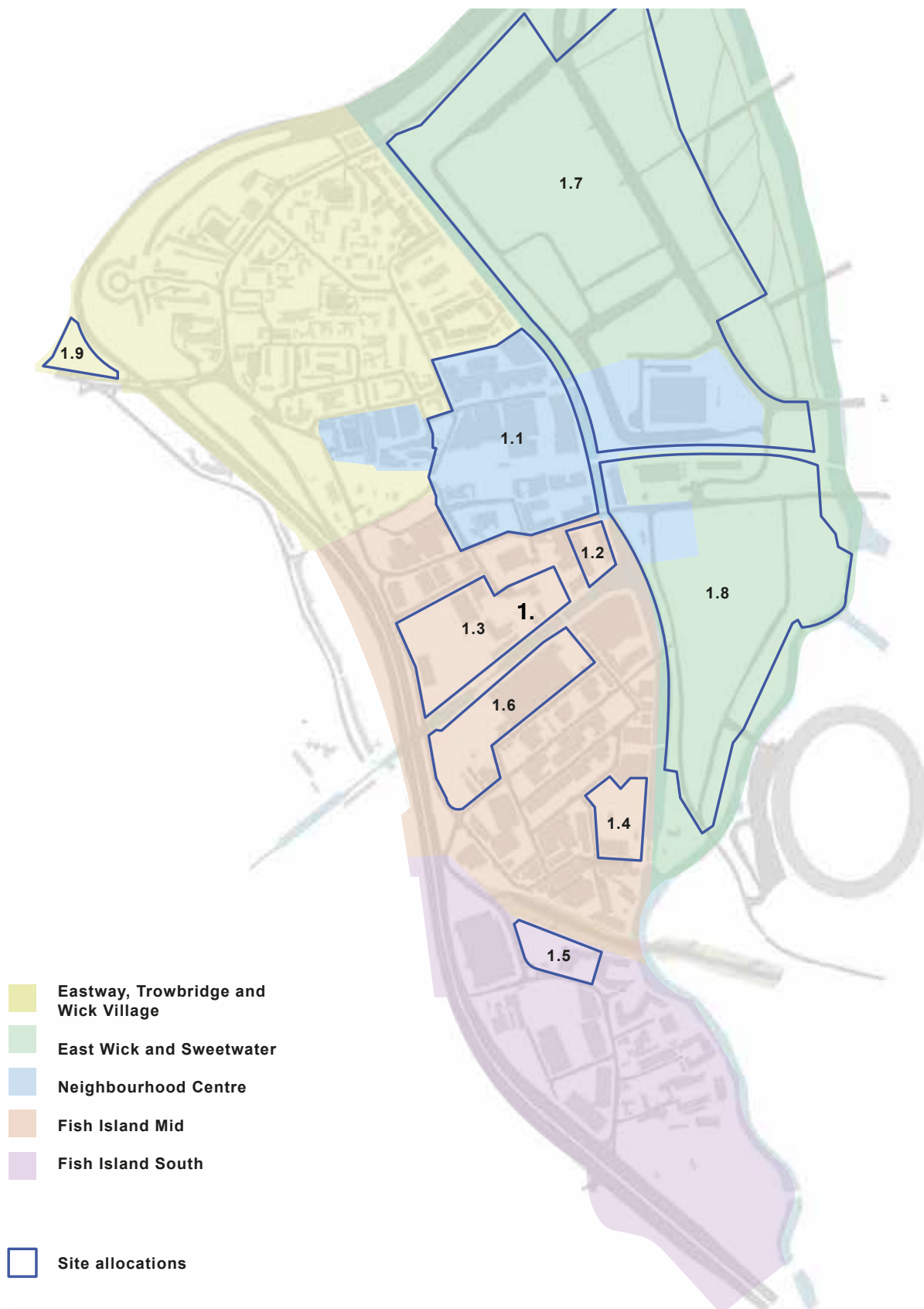
Site Allocation Reference	Site Allocation Name	Local Plan Page Number
SA1.1	Hackney Wick Station Area	164
SA1.2	Hamlet Industrial Estate	166
SA1.3	Hepscott Road	167
SA1.4	Bream Street	168
SA1.5	415 Wick Lane	170
SA1.6	Neptune Wharf	171
SA1.7	East Wick and Here East	172
SA1.8	Sweetwater	173
SA1.9	Bartrip Street South	174

LAND USE
Emerging Distribution



HACKNEY WICK AND FISH ISLAND CHARACTER AREAS

In the following pages this SPD sets out guidance within themed 'Area Wide Priorities' (Section 3) and then more specific guidance for five character areas which are shown on the map below:



3. AREA WIDE PRIORITIES

EMPLOYMENT USES AND LOCATIONS

Employment Strategy

Hackney Wick and Fish Island are a focus for a significant proportion of the projected 59,000 sqm of additional B use classes floor space requirement for the Legacy Corporation area during the Local Plan plan period to 2031. (Note: excludes consented B class space at Here East and Stratford City - sources LLDC Employment Land Review, May 2014).

The strategy for employment uses overall in these areas is to:

- Focus Use Class B1a (office type) use, retail, food and drink, leisure and community use growth within the Neighbourhood Centre alongside new residential development.
- Support development and growth of technology, broadcast and higher/further education use at Here East.
- Encourage a diversification through introduction of new residential development within Fish Island Mid but ensure that employment use/space are retained or reprovided in accordance with the requirements of Local Plan Policy B.1.
- Ensure that Locally Significant Industrial Sites remain in policy compliant industrial use.
- Maintain the industrial character and function of the Strategic Industrial Land (SIL) areas and encourage intensification of compliant uses.
- Maintain the employment use character of the Other Industrial Location (OIL) area while allowing an element of new residential use that facilitates a liveable transition to the SIL area to the south.

Employment Land Designations

A significant element of the land area in Hackney Wick and Fish Island is covered by a Local Plan employment land designation. This includes a significant element of Strategic Industrial Land (SIL), a Other Industrial Location (OIL) and four Locally Significant Industrial Sites (LSIS). Details these can be found at Figure 1 and within the 'Eastway, Trowbridge Estate and Wick Village area specific guidance section. Requirements within the relevant Local Plan policies will be essential components of any successful development scheme in these locations.

Key Employment Policies

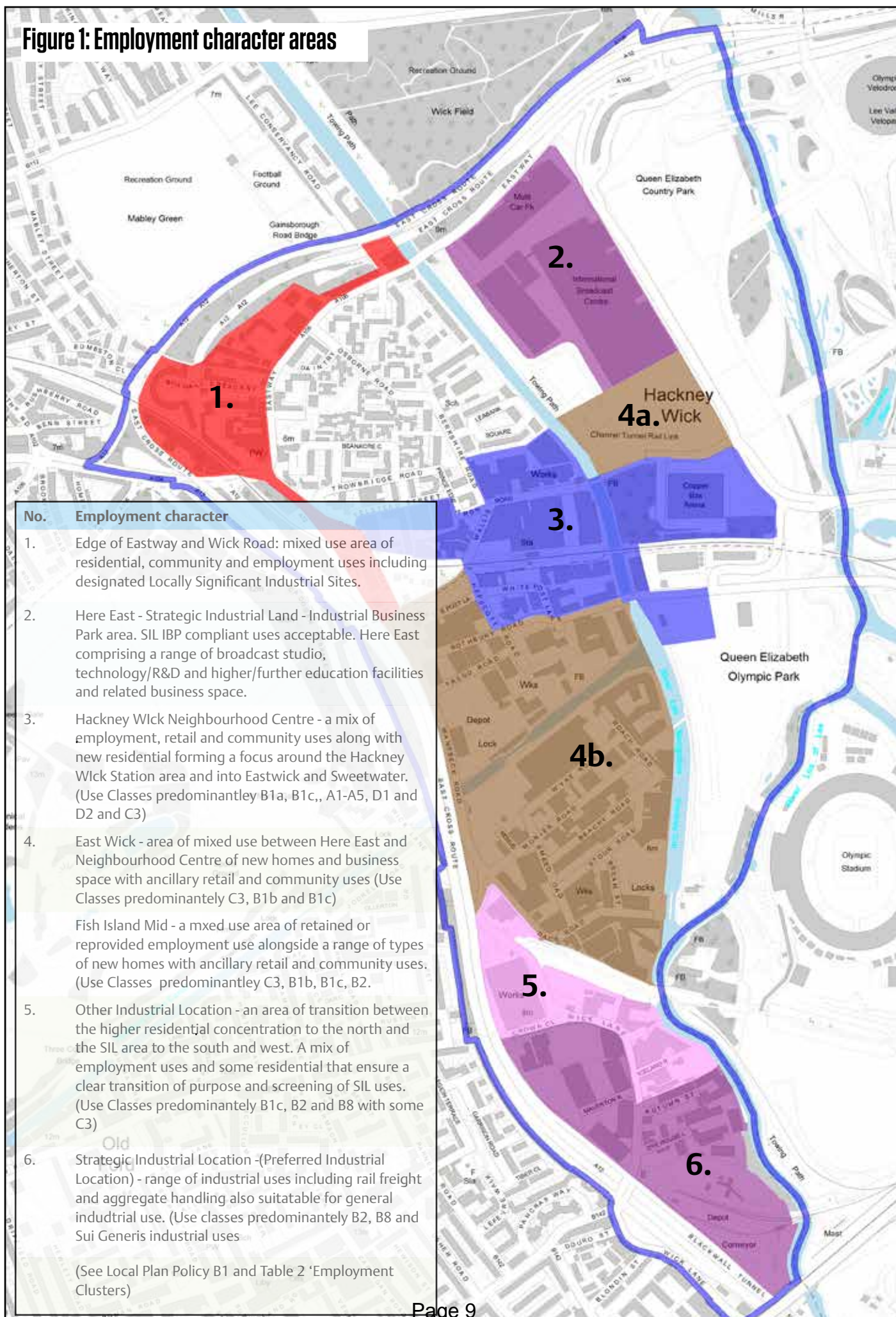
Policy B.1: Location and maintenance of employment uses: sets out the acceptable locations and approaches to B use class uses within the Legacy Corporation area and the acceptable approach to maintaining or reproviding existing floorspace. In conjunction with Local Plan Table 2 (Employment Clusters) and the Policies Map, sets out the employment land designations and appropriate uses for each.

Policy B.2 Thriving town, neighbourhood and local centres: sets out the approach to maintaining and developing town centres through location of appropriate uses, in particular A Use Class retail and D Use Class leisure uses. Identifies in conjunction with Local Plan Table 3 (Retail centre hierarchy) the designation and hierarchy of existing and new centres.

Policy B3: Creating vitality through interim uses: sets out tests for acceptability of interim uses in locations where permanent new development has yet to take place.



Figure 1: Employment character areas



Policy B.4: Providing low-cost and managed workspace: seeks retention of existing space within new development and encourages provision of new low-cost or managed workspace.

Retail and Community Uses

With the designation of a Neighbourhood Centre, it is expected that growth in retail and service uses will take place and contribute to overall growth in employment. A factor in applying the employment policies in the Local Plan, in particular the tests relevant to Policy B.1(5) for maintaining or reproviding relevant B Class Uses, will be the extent to which schemes also contribute to provision of retail, leisure, food and drink, local financial and other services and community uses.

It will be important that schemes achieve a neighbourhood centre that is both liveable and is a practical place to operate a business. A careful approach to design in order to achieve compatibility will be necessary.

Employment use typologies and locations

The 2015 Legacy Corporation Employment Space Study identifies a set of employment typologies within B use classes that are specific and relevant to the Legacy Corporation area. Taking into account the Local Plan strategy for Hackney Wick and Fish Island and the application of its detailed policies and site allocations, the following are considered to be the most appropriate of those typologies within each employment character area. The study has been published alongside this SPD.



Employment Character Area	Generally appropriate typologies (Employment Space Study 2015)
1. Edge of Eastway and Wick Road	Small office space Small creative studio Small industrial/warehouse (LSIS sites) Maker space (LSIS sites) Large industrial space (some LSIS sites) Wholesale/storage space (LSIS sites) Yard space (business yard (LSIS sites))
2. Here East	Small office space Standards large office Managed workspace Incubator/accelerator/co-working space Bespoke premises Small creative studio Large creative studio Small industrial space Large industrial Maker space
3. Hackney Wick Neighbourhood Centre	Small office space Standards large office space Managed workspace Incubator/accelerator/co-working space Small creative studio Large creative studio Small industrial space Maker space (part of an employment mix that includes retail, leisure and community facility space (Use Classes A1-A5; D1; D2))
4. Fish Island Mid	Managed workspace Incubator/accelerator/co-working space Small creative studio Large creative studio Small industrial space Maker space
5. Other Industrial Location	Managed workspace Incubator/accelerator/co-working space Small creative studio Small industrial space Large industrial Wholesale/storage space Maker space Yard space (business yard)
6. Strategic Industrial Location (Preferred Industrial Location)	Small industrial space Large industrial Wholesale/storage space Yard space (business yard)
Note: this table outlines the generally appropriate employment space typologies for each employment character area as identified in the LLDC Employment Space Study (2015). For specific policy requirements and uses reference should be made to Local Plan Policy B.1 and table 2 (Employment Clusters) within the Local Plan 2015-2031.	

Compatibility of uses

Local Plan Policy B.1(5) includes provision for circumstances in which an existing employment use would be incompatible with an otherwise acceptable development scheme. Where such a situation exists and the other requirements of this policy have been met, consideration of a relocation of that use is required. Locations or sites suitable for such incompatible uses are most likely to be those with an existing employment land designation, particularly Strategic Industrial Land (SIL), including the Fish Island South SIL area, and the Locally Significant Industrial Sites (LSIS).

Relocation strategies

Local Plan Policy B.1(5) also seeks the preparation and use of relocation strategies in circumstances where an existing employment use may be lost through redevelopment. Appendix 1 of this SPD sets out guidance on the approach and content of relocation strategies.

Note on Live/Work Accommodation

Live/Work accommodation is not a form of provision that is supported by policy in the Local Plan. Where proposals for Live/Work accommodation are included within development schemes these will need to meet the employment and housing policy requirements within the Local Plan. Live/Work space is likely to be treated as a “sui generis” use.

The following is guidance on adopted policy requirements that would apply to any development proposal that included live/work accommodation. Such proposals would need to:

- ensure the requirements of Policy B.1 are met in terms of retention or re-provision of business space within the scheme (i.e. existing amounts of floorspace maintained or re-provided unless the relevant tests within the policy have been robustly met);
- ensure that the work element of space remained in business/non-residential use to continue to meet the requirements for retention or re-provision, which could include having suitable on-going management arrangements;
- ensure that living accommodation meets the requirements of housing and design policies (including the requirement for affordable housing provision).

- meet the requirements of other regulatory regimes, e.g. Building Regulations and Fire Regulations.

Schemes that meet the required Local Plan policy tests and also physically separate living and working accommodation within the scheme while linking these through lease or tenancy arrangements may provide a greater level of flexibility and be better able to meet the policy tests.



Existing businesses in Hackney Wick and Fish Island

The LLDC Economy Study Part C (Qualitative Research) report and study by We Made That in 2014, identified a range of example businesses within the Legacy Corporation area, with a focus on Hackney Wick and Fish Island. The report helps to characterise a range of employment types that contribute to the areas economy. The following are examples taken from this as local businesses at the time.

Trumans Brewery, Stour Road, Fish Island

Truman's Brewery was originally established in 1666. Over more than 320 years, it became one of the best known names in brewing across London, particularly associated with the east. Truman's was re-established in Stour Road, and has since experienced rapid (re)growth. Premises are held on a 6 year lease. Building size: 5,000 sq ft (460 sq m), with further 5,000 sq ft external yard. Staff: Currently 16 employees, expected to increase.

Suppliers: Kent, East Anglia, Scotland, London.

Customers: Predominantly East London.



Bamboo Bicycle Club, Autumn Street

The UK's first 'build your own' bicycle bamboo workshop community, launched in September 2012. Bamboo Bicycle Club runs weekend workshops where people learn and build their own bike frames made from bamboo. Responding to an emerging trend of interest in self-made cycles, Limited company established in 2012. Moved in to the area in 2012, and into the current premises in 2013. Premises sub-let from leasehold tenants. Building size: 650 sq ft (60 sqm). Staff: 1 director working full time, with three part time interns.

Suppliers: Bamboo supplier based in Tunbridge Wells, sourced from all over the world. Some bespoke parts fabrication in Dorset.

Customers: From across the UK and Europe, including some international attendees.



E. Abrahams & Co, Crown Close (Off Wick Lane)

Manufacturers of wooden packing cases, warehouse containers and fine art cases since 1865. E. Abrahams & Co is a family business, owned and managed by relations of the original founder. The company offers a complete bespoke service for crates of a range of sizes and specifications, available on a next-day service. Originally established in 1865 and incorporated in the 1920s. Building size: 5,300 sq ft (495 sqm); internally, plus further 5,590 sq ft (520sqm) externally. Staff: 11 staff,

Suppliers: Pine from Scandinavia, plywood from China and Malaysia, other sundries from across the South East of England.

Customers: London and the UK.



Here East

At a different scale, Here East at Hackney Wick, to the east of the Lee Navigation, is projected to provide over 5,000 jobs. The former Olympic Broadcast and Press Centres buildings are being transformed into a range of tech and creative industry focused studios and spaces for large and small businesses along with an education focus that includes Loughborough University, University College London and Hackney Community College. With some business already in occupation, the conversion of the buildings is due to be completed during 2016, with Here East becoming the main employment hub within the west of the Legacy Corporation area.



HOUSING

Housing Strategy

- At least 4,400 new homes
- Maximising affordable housing
- Protecting existing residential communities (Eastway, Trowbridge, Wick Village)
- East of the Lea Navigation (Hackney Cut) - delivering the new homes at Eastwick and Sweetwater
- West of the Lea Navigation (Hackney Cut) - a mix of new homes and business uses in the Neighbourhood Centre and Fish Island-mid.
- Protecting existing and delivering new Gypsy and Traveller accommodation

Key Housing Policies

Policy H1: Providing a mix of housing types.

contain a mix of one, two and three bedroom units (units of two bedrooms or more consisting of more than half the mix), must integrate a mix of housing unit types.

Policy H.2: Affordable Housing

maximise the amount of affordable housing on sites of 10 units or more (60% affordable/social rent and 40% intermediate).

Policy H.7: Large-scale private rented housing

Treated as market housing

Must include and maximise affordable housing in accordance with Policy H.2

Include a mechanism to secure accommodation as private rented in the long-term.

Housing locations

1. Neighbourhood Centre

new residential development mixed with new and retained employment space, retail and community uses with potential for higher densities inside the centre boundary where compatible with the Conservation Area Designation.

2. Mixed use areas

Areas of new residential development providing a range of unit types alongside and over new and retained employment uses (typically Use Class B1c, B2 and B8), including within retained heritage buildings and respecting existing street patterns and character.

3. Established residential and mixed use areas

Established residential areas with established communities with only small scale change overtime. Includes Trowbridge Estate and Wick Village, with

bordering area along Eastway and Wick Lane continuing to provide a range of established residential and community uses with isolated sites designated as and continuing to function as 'Locally Significant Industrial Sites' (LSIS).

4. New residential areas

New residential areas of Eastwick and Sweetwater delivered as part of the Legacy Corporations Legacy Communities Scheme, with minor ancillary elements of community, retail and employment space and two primary schools.

5. Residential in the Other Industrial Location

A limited amount of residential use seen as acceptable where this successfully allows for a transition of use and environment between the Greenway and Fish Island-mid to the north and the Strategic Industrial Land (SIL) to the south.

6. Gypsy and traveller accommodation

Protection of use at existing Palace Close/St.

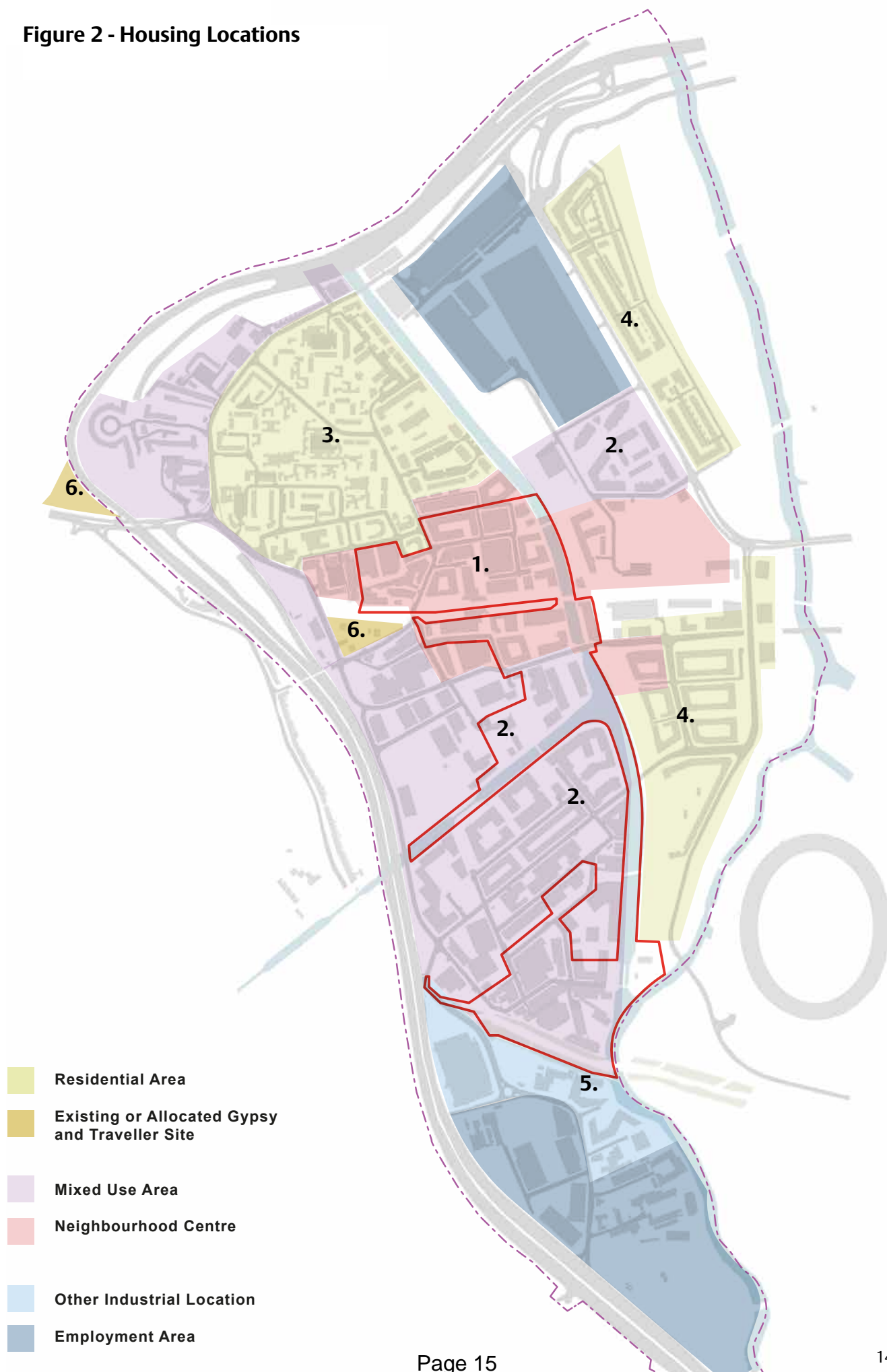
Anthony's Close site, while seeking delivery of new site in line with the Local Plan SA1.9 Bartrip Street Site Allocation.

Effective delivery of affordable housing

With many smaller sites a characteristic of the Neighbourhood Centre and Fish Island Mid areas, developers are encouraged to cooperate when planning affordable housing delivery. A shared approach to securing arrangements with registered providers may help to achieve economies in delivery and management of affordable housing.



Figure 2 - Housing Locations



INFRASTRUCTURE - CONNECTIVITY

Connectivity Strategy

With the strategic transport infrastructure in place and improvements to Hackney Wick Station secured the focus for the area is primarily the delivery of new or improved walking and cycling routes and an accessible public realm that will in turn improve access to public transport.

Key Policies

Policy T4 Transport Choices and Local Connectivity: designing new development to minimise the demand for private car use, facilitate access to public transport and encourage walking and cycling.

Policy T6: Facilitating Local Connectivity: the need for new development to facilitate local connectivity and prioritise walking and cycling.

Policy T.6 Providing for Pedestrians and Cyclists: principles and standards for walking and cycling infrastructure.

Policy 1.3 Connecting Hackney Wick and Fish Island: principles for routes provided through and by new development, including those through yards and building plots.

Key interventions and connectivity projects

Nine key improvements to connections within Hackney Wick and Fish Island have been identified as important for achieving the connectivity improvements. Each of these are identified in principle as improvements in the Local Plan and as

project within the Infrastructure Delivery Plan projects list.

1. H10 Wallis Road/Lee Navigation Bridge - cycling improvements
2. North South Route (Hackney Wick Station) Prince Edward Road to White Post Lane link, including underpass through railway embankment.
3. North South Route (Hertford Union Canal) including replacement pedestrian and cycle bridge to Roach Road.
4. A12 Wallis Road footbridge - improvements to existing pedestrian and cycle connection.
5. Wansbeck Road - improved pedestrian and cycling provision along the highway/footpath.
6. H14 Bridge, Monier Road to Sweetwater - replacement of pedestrian and cycle bridge with an all modes bridge.
7. H16 Beechy Road new pedestrian and cycle bridge.
8. Greenway Connection - pedestrian and cycle link from the Greenway to Wick Lane.
9. Old Ford Road Bridge - A12 Crown Close to Roman Road link - replacing pedestrian and cycle bridge to provide all modes link from Roman Road to Crown Close/Monier Road.

Public transport

The key public transport intervention is the improvement planned to Hackney Wick Station.

Current bus routes that service Hackney Wick and Fish Island are:

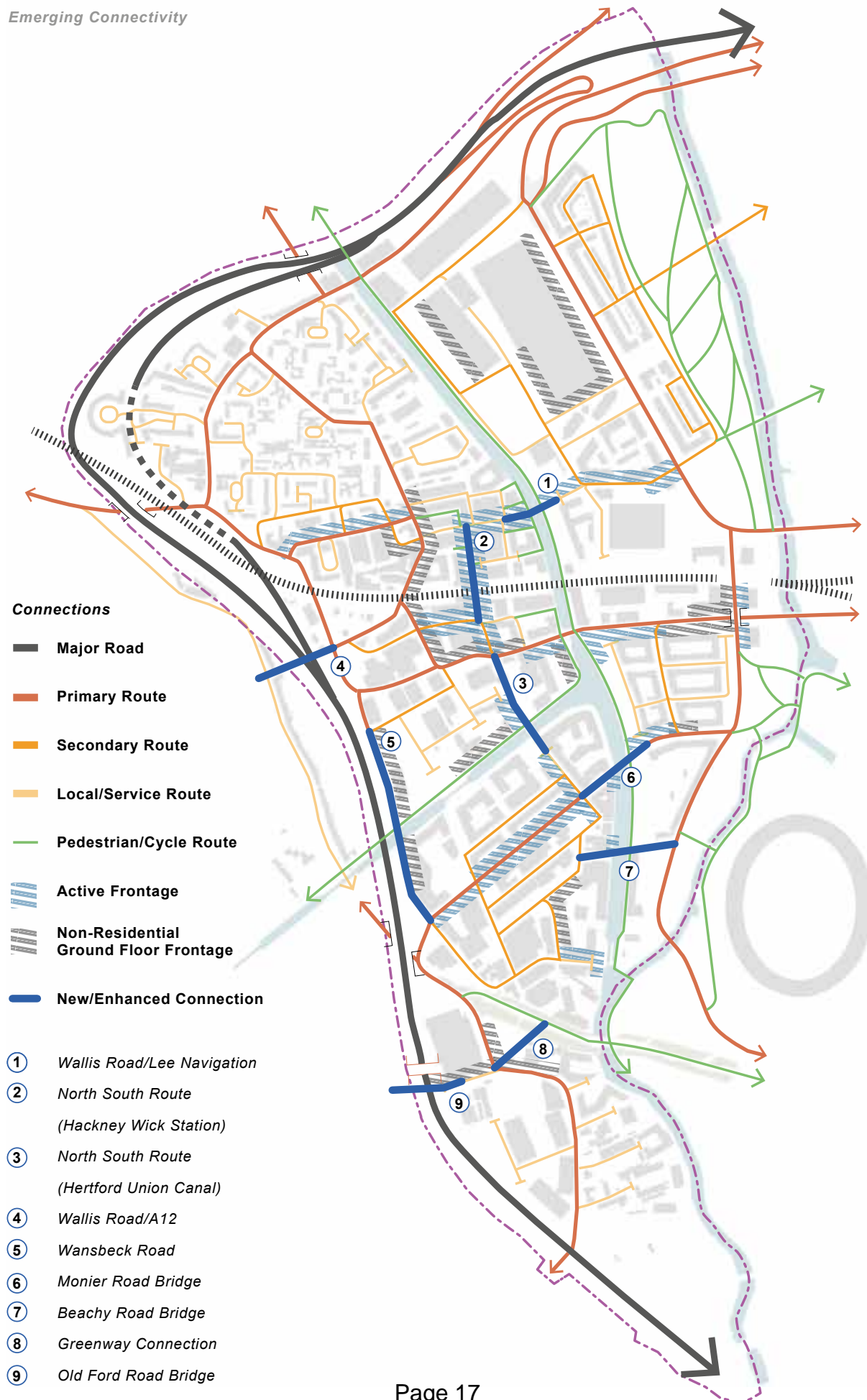
- Route 30 - Oxford Street to Hackney Wick (Eastway)
- Route 488 - Bromley-by-Bow to Kingsland Rd, Hackney
- Route 388 - Blackfriars Stn to Stratford
- Route 8 - Tottenham Court road to Roman Road

The future delivery of Bridge H14 replacement as bridge capable of providing a future bus route via Monier Road and Sweetwater. These routes within the Hackney Wick and Fish Island area will require ongoing review with TfL to ensure that they continue to serve this locality appropriately as the residential population changes and grows.



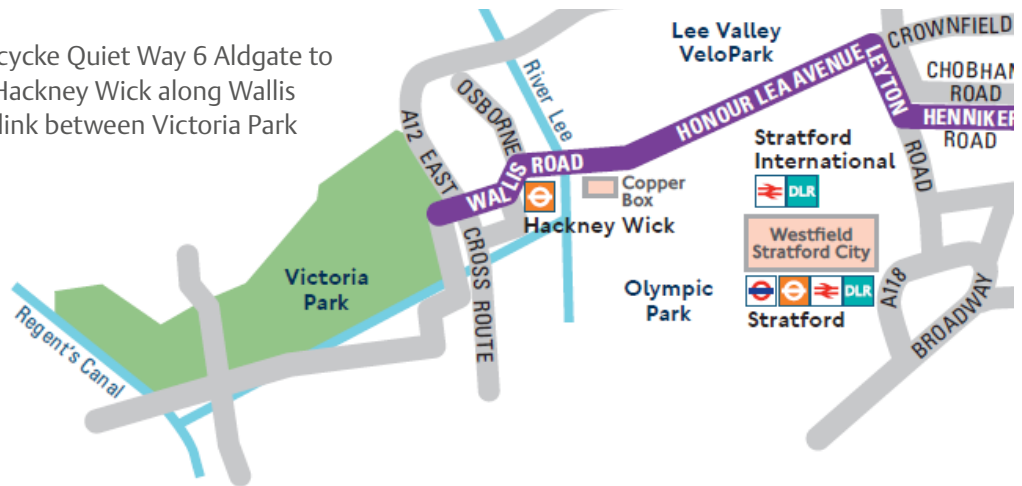
ROUTE NETWORK

Emerging Connectivity



Quiet Way 6

It is worth noting that cycle Quiet Way 6 Aldgate to Hainault runs through Hackney Wick along Wallis Road providing a local link between Victoria Park and East Village.



Public Transport Accessibility Levels

Figure (X) shows the current TfL Public Accessibility Levels (PTALs) for the Hackney Wick and Fish Island area, while Figure (XY) shows the projected PTALs levels for the area by 2031. While only one indicator in interpreting policies for development density and levels of parking, the PTALs levels along with other relevant factors will help to define the approach likely to be acceptable in specific development proposals.

Map key - PTAL



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Crossrail 2 - Eastern Phase

The proposed route for Crossrail 2 includes a branch via Hackney and Tottenham Hale, with the intention that the scheme is delivered by 2030. While not included in the formal proposal, an eastern branch to Crossrail 2 is being promoted by the boroughs of Hackney, Newham and Barking *Dagenham along with Essex County Council. Thus has the potential for a route via Hackney Wick, with possibility of stations at Hackney Wick and/or Stratford. While this has no direct impact on the guidance or development outcomes sought within this SPD, a future review of the Local Plan would reflect any firm proposals and route safeguarding that resulted from this.



INFRASTRUCTURE - HEATING AND COOLING

Strategy

To build on the opportunities presented by the existing heating and cooling network and any new network(s) within the area to provide for the heating and power needs of new and existing development in a way that helps to reduce the associated carbon emissions.

Key Policies

Policy S.2 Energy in new development: sets out approach and requirements for efficient energy provision and reducing carbon emissions, including carbon targets and carbon offsetting.

Policy S.3 Energy infrastructure and heat networks: sets out approach to providing new heat network infrastructure and seeks maximisation of opportunities for connection to heating and cooling networks. Inclusion of network ducting within new bridges.

Policy S.4 Sustainable design and construction: requires non-domestic space to achieve BREEAM Very Good including a maximum score for water use, (or any future equivalent to this standard).

Sequential approach to carbon reduction

- Inclusion of on-site design measures to reduce

energy demand

- Inclusion of on-site renewable energy generation where there is potential/opportunity.
- Extension of existing or creation of new heat networks across waterways and other infrastructure barriers to maximise the number of developments/dwellings and businesses served

Key Priorities

- Use of heating and cooling networks to reduce carbon emissions
- Enabling extension of the existing heat network through provision/adaptation of bridges and use of streets and other utility corridors.
- Cooperation between landowners and others to achieve delivery of space for plant and equipment (e.g. CHP plant, heat exchangers) within new development schemes that are capable of onward network extensions and identification of heat pipe routes for that development.

Heat Network Options

Four options for extension of the existing or creation of a new heat network have been identified for the SPD area, noting that East Wick, Sweetwater and Hear East will be served by the existing heat network centred on the Kings Yard Energy Centre. The options have been identified within the Hackney Wick & Fish Island District Heating Study (April 2016) carried out for the Legacy Corporation by ARUP and published alongside this SPD.

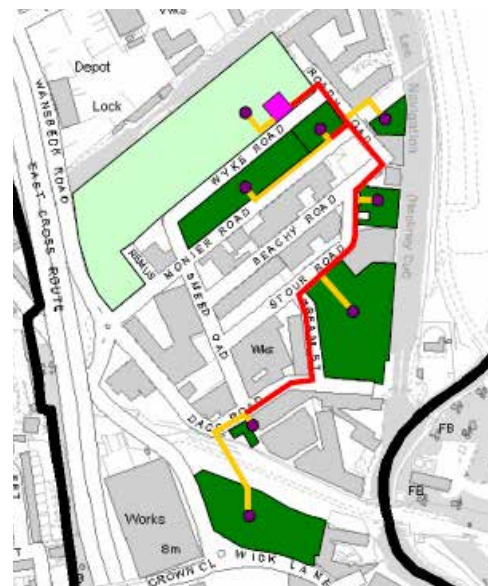
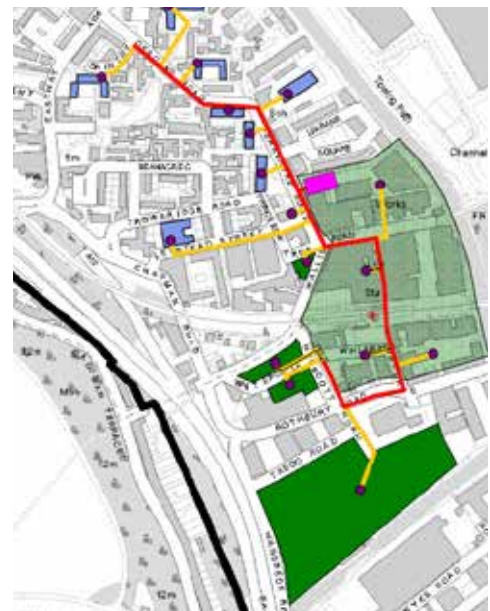
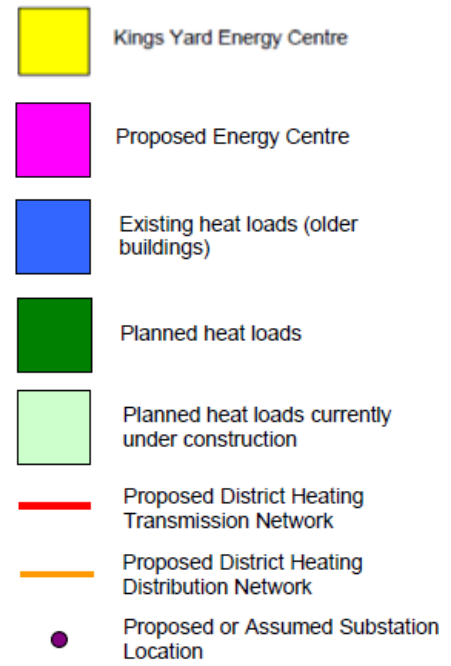
Option 1: connection to existing heat network crossing the Lee Navigation to provide a heat network to Hackney Wick and Fish Island Mid. Heat network bridge crossings at one or more bridge (White Post Lane Bridge and Bridge H14, Bridge H16). This is the preferred option.

Option 2: Standalone Hackney Wick and Fish Island heat network, to the west of the Lee Navigation, connected to an energy centre within a development plot in the Hackney Wick Station area. Includes heat Network bridge crossing as part of replacement Hertford Union Canal Bridge.

Option 3: Standalone Hackney Wick heat network, with energy centre within a development plot forming part of the Hackney Wick Station area.

Option 4: Standalone Fish Island heat network, utilising planned energy centre for the Neptune Wharf Scheme.





INFRASTRUCTURE - SCHOOLS, LIBRARIES, HEALTH FACILITIES, MEETING SPACE

Strategy

Work towards delivery of identified required social infrastructure to meet the needs of the new and existing population in the area as part of new development schemes and coordination with infrastructure providing bodies and funding agencies.

Key Policies

Policy CI.1 New and existing community infrastructure: protects/requires replacement of existing community infrastructure unless loss is specifically justified. Seeks shared/alternative use of community infrastructure where this is appropriate.

Policy CI.2 Planning for schools. Sets out policy requirements for new school proposals.

Key Priorities

Delivery of the identified new schools and other social infrastructure (see Table 1 below). Map Y on page X indicates the location of existing and planned community infrastructure.



Table 1: Planned new social infrastructure

Type	Location	Delivery Agency
Library/Ideas Store	White Post Lane, Sweetwater, Fish Island	LLDC
Medical Facility	Sweetwater, Fish Island	LLDC/relevant GP commissioning group
East Wick Primary School	East Wick, Hackney Wick	LLDC/Mossbourne Academy
Sweetwater Primary School	Sweetwater (south), Fish Island	LLDC/Legatum Academy
Neptune Wharf Primary School	Monier Road, Fish Island	Developer/LB Tower Hamlets



PUBLIC REALM, OPEN SPACE AND WATERWAYS

Public Realm, waterways and open space strategy

- Create a public realm that is coherent, robust, simple, accessible and safe. with a focus on re-use of materials.
- Stich together the Queen Elizabeth Olympic Park and the wider area of Hackney Wick and Fish Island, providing linages to Vlctoria Park and other strategic open spaces.
- Improve and enhance the role of the waterways for drainage, biodiversity , leisure and transport.
- Ensuring that the strategic and local role of the green corridors are protective and enhanced.
- Protecting and enhancing the existing open and green spaces for biodiversity, leisure and play.
- Delivery of new open spaces identified within Local Plan site allocations.
- Appropriate greening of the public realm and buildings.

Key Local Plan Policies

BN.1 Responding to place - development needs to relate well to the area's natural and man made lanscape features, including linear form of the parklands and waterways.

BN.2 Creating distinctve waterway environments - improve ecological potential, drainage and flood resilience capacity, create opporutuities for recreation; introduce new and improve existing residential, commercial and visitor moorings; improve access.

BN.3 Maximising biodiversity - provide net gain in extent of and integrate habitat to support biodiversity; conserve and enhance SINC's; retain and plant new trees; take account of BAP habtat and species target; avoid breakage of habitat or wildlife corridors.

BN.6 Protecting Metropolitan Open Land (MOL) - development on MOL only acceptable in 'very special circumstances'.

BN.7 Improving Local Open Space - protect and enhance function, quality, character of existing Local Open Space; consideration of inclusion of new open space in major development schemes where a quantative or qualitative deficiency has been identified. Loss of open space requiring equivalent localised compensatory provision.

BN.8 Maximising opportunities for play -improve or provide new play spaces in acordance with London Plan Policy 7.5 and the Mayors Shaping Neighbourhoods Play and Informal Recreation SPG)

T.10 Using the waterways for transport - ensourage use of the waterways for freight, leisure and passenger transport while balalancing this use against biodiversity and drainage functions.

S.1 Health and wellbeing - major development schemes required to show how they contribute to the health and wellbeing of occupants and existing residents and are designed to encourage active and healthy lifestyles.

S7 Overheating and urban greening - use of tree and other planting, use of green roofs and green walls to maximise urban greening andd help mitgate potential effects of overheating in the urban environment and within buildings.

Locations and Designations

Metropolitan Open Land areas - Queen Elizabeth Olympic Park and the Greenway

Local Open Spaces - includes all MOL designated space and additional spaces indicated on opposite page.

New open spaces - Associated with the site allocations for SA1.3 Hepscott Road;S A1.5 Wick LaneSA1.6 Neptune Wharf;

Blue Ribbon Network - all waterways within the sub area form part of the London Blue Ribbon Network (London Plan Policies 7.4-7.30)

Green Grid - the main parkland areas and open spaces are a part of the London Green Grid. (Mayor of London All London Green Grid SPG, 2012)

Waterways as SINC's - The Hackney Wick and Fish Island waterways are designated as Sites of Importance for Nature Conservation (SINC's).

PUBLIC REALM, OPEN SPACE AND WATERWAYS

Public Realm, waterways and open space strategy

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Key Local Plan Policies

BN.1 Responding to place - development needs to relate well to the area's natural and man made lanscape features, including linear form of the parklands and waterways.

BN.2 Creating distinctive waterway environments - improve ecological potential, drainage and flood resilience capacity, create oppotunities for recreation; introduce new and improve existing residential, commercial and visitor moorings; improve access.

BN.3 Maximising biodiversity - provide net gain in extent of and integrate habitat to support biodiversity; conserve and enhance SINC's; retain and plant new trees; take account of BAP habtat and species target; avoid breakage of habitat or wildlife corridors.

BN.6 Protecting Metropolitan Open Land (MOL) - development on MOL only acceptable in 'very special circumstances'.

BN.7 Improving Local Open Space - protect and enhance function, quality, character of existing Local Open Space; consideration of inclusion of new open space in major development schemes where a quantative or qualitative deficiency has been identified. Loss of open space requiring equivalent localised compensatory provision.

BN.8 Maximising opportunities for play -improve or provide new play spaces in accordance with London Plan Policy 7.5 and the Mayors Shaping Neighbourhoods Play and Informal Recreation SPG)

T.10 Using the waterways for transport - encourage use of the waterways for freight, leisure and passenger transport while balancing this use against biodiversity and drainage functions.

S.1 Health and wellbeing - major development schemes required to show how they contribute to the health and wellbeing of occupants and existing residents and are designed to encourage active and healthy lifestyles.

S7 Overheating and urban greening - use of tree and other planting, use of green roofs and green walls to maximise urban greening andd help mitigate potential effects of overheating in the urban environment and within buildings.

Locations and Designations

Metropolitan Open Land areas - Queen Elizabeth Olympic Park and the Greenway

Local Open Spaces - includes all MOL designated space and additional spaces indicated on opposite page.

New open spaces - Associated with the site allocations for SA1.3 Hepscott Road;S A1.5 Wick LaneSA1.6 Neptune Wharf;

Blue Ribbon Network - all waterways within the sub area form part of the London Blue Ribbon Network (London Plan Policies 7.4-7.30)

Green Grid - the main parkland areas and open spaces are a part of the London Green Grid. (Mayor of London All London Green Grid SPG, 2012)
Waterways as SINC's - The Hackney Wick and Fish Island waterways are designated as Sites of Importance for Nature Conservation (SINCs).





OPEN SPACE AND WATERWAYS

Emerging Distribution

Community Infrastructure

-  Further Education
-  All Through School
-  Secondary School
-  Primary School
-  Nursery School
-  Medical Facility
-  Community Sport
-  Faith-based Space
-  General Community Facility
-  Play Space

Local Open Space

-  Green Corridor
-  Public Open Space/Park
-  Private or Communal Garden
-  Proposed Local Open Space

Strategic green corridor linkages

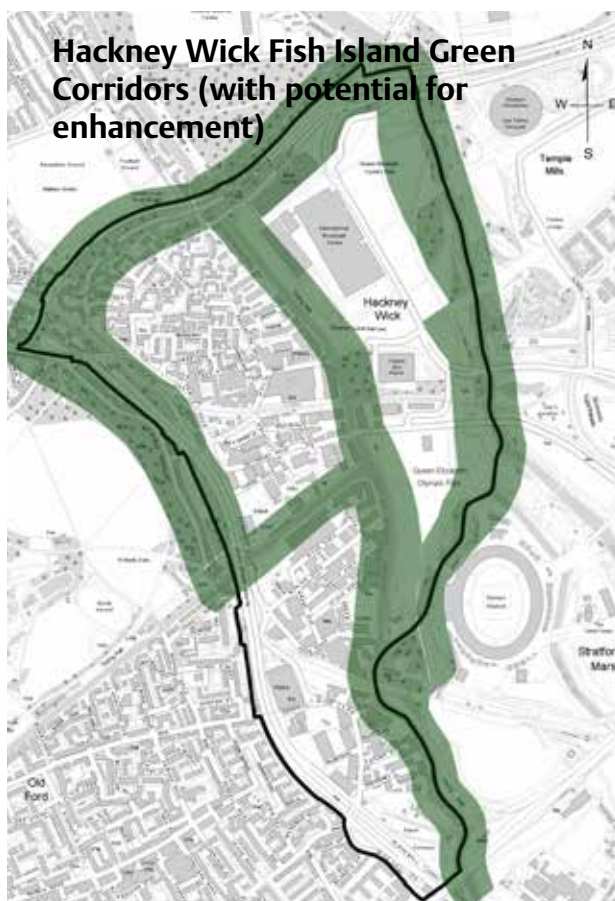
Hackney Wick and Fish Island along with the Legacy Corporation area as a whole within the Lower Lea Valley, providing a strategic link from the upper Lee Valley to the north of London to the River Thames at its confluence with the River Lee. More locally the component green assets of Hackney Marshes to the north, Queen Elizabeth Olympic Park to the east and Victoria Park to the west make the Hackney Wick and Fish Island area an important opportunity for enhancement of the urban green linkages between these spaces.



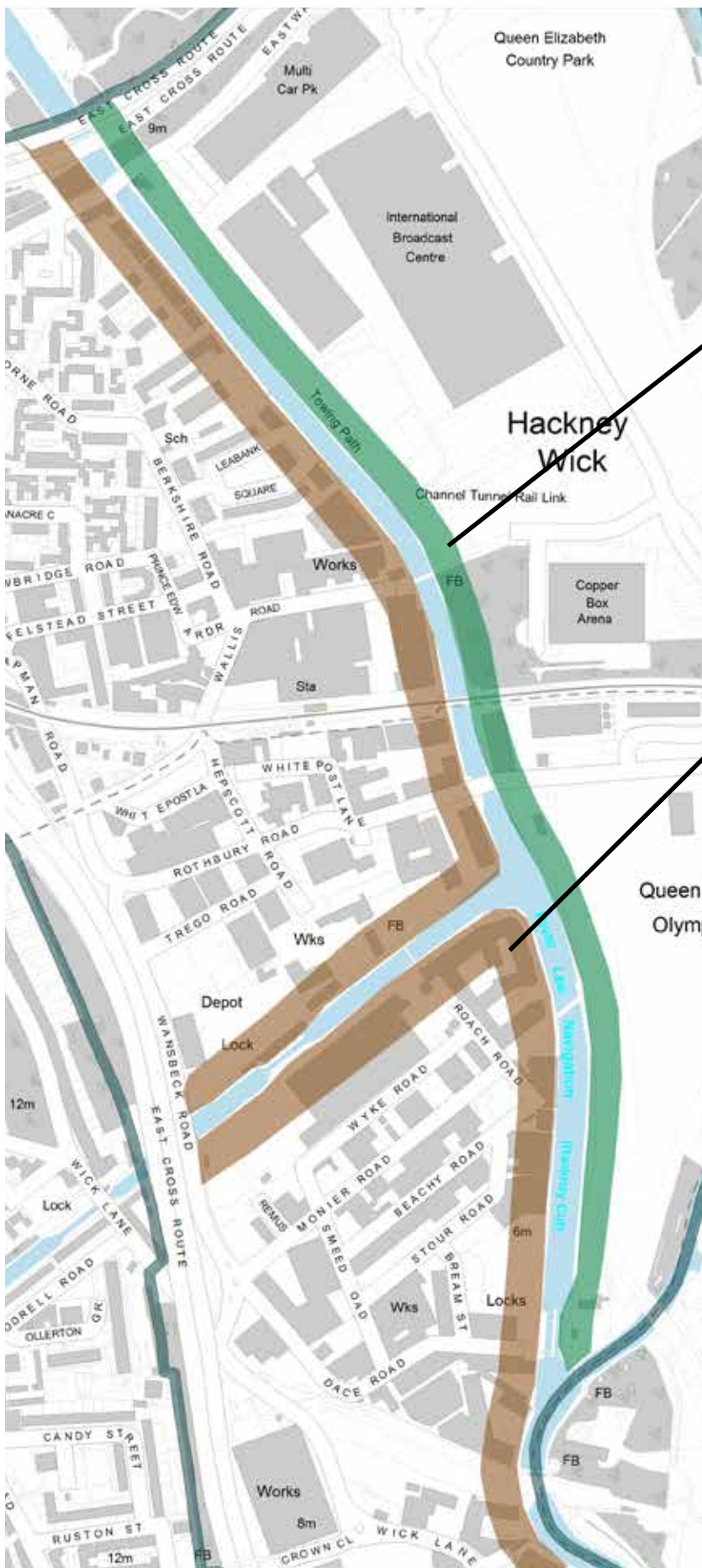
Enhancing Green Corridors

The waterways, parkland and other green edges within and around Hackney Wick and Fish Island present the opportunity to enhance the local wildlife and habitat linkages that these create. Maintenance of the high quality habitat areas along with opportunities to expand and improve these corridors are present through:

- appropriate waterway edge treatment including planting
- Setting new development back from the waterway edge/tow path
- location of new public and private spaces within new development within the corridors
- concentrating green roofs within new development along these corridors
- general use of street trees and other urban greening measures within the public realm and on buildings
- Using locally appropriate planting species within open spaces, public realm and on green roofs (see relevant biodiversity action plans for target habitats and species).
- Balance habitat creation with the commercial, leisure and drainage functions of the waterways.



(See Local Plan Policies BN.2 Creating distinctive waterway environments; Policy BN.3 Maximising biodiversity; Policy S.4 Sustainable design and construction; Policy S.7 Overheating and urban greening.



Eastern Bank interface

Route of towpath, Canal Park and other publicly accessible spaces with opportunities for compatible canalside activity and maintenance of habitat and species rich environment. Buildings generally away from canal edge to maintain environmental buffer and movement corridors.

Western Bank interface

Mix of public and private waterway frontage, including some towpath area. Buildings close to canal edge in places. Provides potential for further opening up of waterway frontage, harder landscaping types but with opportunities for greening in parts.

YOUTH SPACE

Youth space

A social space for young people aged 12+ to congregate together, socialise and participate in informal recreation or physical activity.

GLA Play and Recreation SPD 2012

In terms of both facilities and activities 'youth space' is perhaps less simply definable, and therefore less easily delivered as part of new residential development, than play space for children of younger years.

As part of the development of Queen Elizabeth Olympic Park, a number of youth play spaces were delivered, such as the climbing wall below. However, industrial and ex-industrial areas around the Park - Fish Island, Pudding Mill Lane, and Bromley by Bow - suffer from deficiencies in youth space, as well as deficiencies in open space and play space in general. These areas currently have low, or no residential populations, but with the amount of development planned for each, consideration of opportunities for enhanced or new provision is required as schemes come forward.

Principles of good youth space

- The space is used by children of all ages
- It considers the interests and habits of both girls and boys
- The space is well integrated into its surroundings, with good natural surveillance: it feels safe
- Young people are part of the design process
- The design is imaginative and bespoke
- The design employs natural materials and engages with the landscape
- There is an element of challenge or risk

Projects to mitigate the deficiency of youth space

Site	Delivery	Status
1. East Wick Youth Play Space	Part of the East Wick Sweetwater development	Planned
2. Canal Park MUGA	Part of the East Wick Sweetwater development	Planned
3. Sweetwater Youth Play Space	Part of the East Wick Sweetwater development	Planned
4. Pudding Mill Neighbourhood Space	Part of the Pudding Mill development	Planned
5. Abbey Green MUGA	LB Newham	Planned - subject to partnership funding
6. Rick Roberts Way Youth Space	LB Newham/ LLDC	Recommended
7. Hackney Wick South	Developer led	Recommended
8. Pudding Mill East/3A3.5 (Bridgewater Road)	Part of LCS development	Recommended
9. Fish Island Youth Space	Developer led	Recommended
10. Bromley-by-Bow	Developer led	Recommended
11. TIQ/Westfield	Developer led	Recommended
12. Eton Manor Programming	Borough/ developer led	Recommended
13. Three Mills Green Programming	Borough/ developer led	Recommended
14. Frontside Garden & Hub 67 (existing temporary youth facilities)	LLDC/ developer led relocation	Recommended

Planned/recommended for Hackney Wick and Fish Island



Cowley Teenage Space
Lambeth

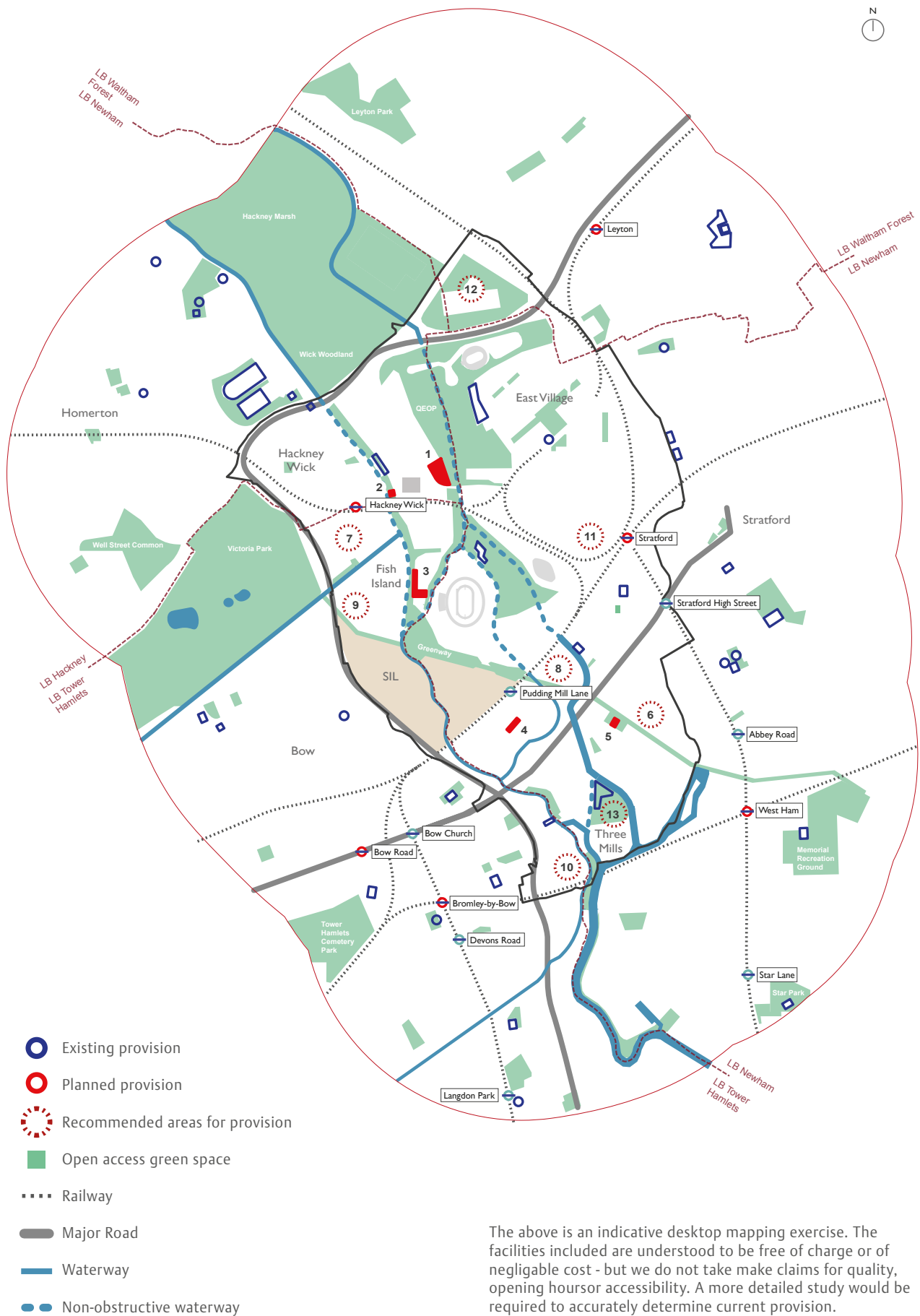


Climbing Wall
Queen Elizabeth Olympic Park



Frontside Garden
Hackney Wick

LOCAL PROVISION OF YOUTH SPACE



FLOODING AND DRAINAGE

Flood Risk in Hackney Wick and Fish Island

Risk of flooding, both fluvial flooding from the River Lea and Lea Navigation and surface water flooding, present a significant challenge for new development in Hackney Wick and also the southern part of Fish Island Mid. Proposals for new development will need to take account of that flood risk in how they are designed and also ensure that flood risk levels are not worsened.

Flood Risk Policy

Policy S.8 Flood risk and sustainable drainage measures (key areas):

Requirements for flood risk assessment, levels of finished floor levels, avoiding basements in Flood Zones 2 and 3, and provision of means of escape. Avoiding basement development within Flood Zones 2 & 3. Protection and improvement of flood defences, inclusion of watercourse buffer strips and use of sustainable drainage techniques. Taking account of local drainage problems and potential for flooding from sewers. Inclusion of anti-pollution measures within drainage.

Guidance Principles (development within Flood Zones 2 & 3):

- A site specific Flood Risk Assessment would be required.
- In Flood Zone 2, setting living accommodation at 300mm above predicted level for the 1 in 100 chance in any year flood event (including an allowance for climate change).
- In Flood Zone 3, all residential accommodation to be located on first floor or above.
- Development within areas shown to be at 'actual' risk of flooding may require compensatory storage to demonstrate there will be no loss of floodplain storage.
- Development layout must consider surface water flow routes and manage runoff on site sustainably with a target to achieve Greenfield runoff rates.
- Include 'at source' SUDS control measures to reduce existing site runoff in accordance with London Plan and local policy.
- Open up river corridors and provide more floodplain storage.
- Incorporation of flood resistance / resilience measures up to the flood level.
- Application of the sequential approach at site level to be carried out to ensure 'more vulnerable' land uses are located within Flood Zones 1 and 2.

- Consideration of safe access / egress from the site and safe refuge.
- Implementation of a flood emergency plan.
- Design of development to consider mitigating the risk of groundwater flooding with use of impermeable materials.
- The detailed guidance within the LB Hackney and LB Tower Hamlets SFRA's for Hackney Wick and for Fish Island should also be taken into account

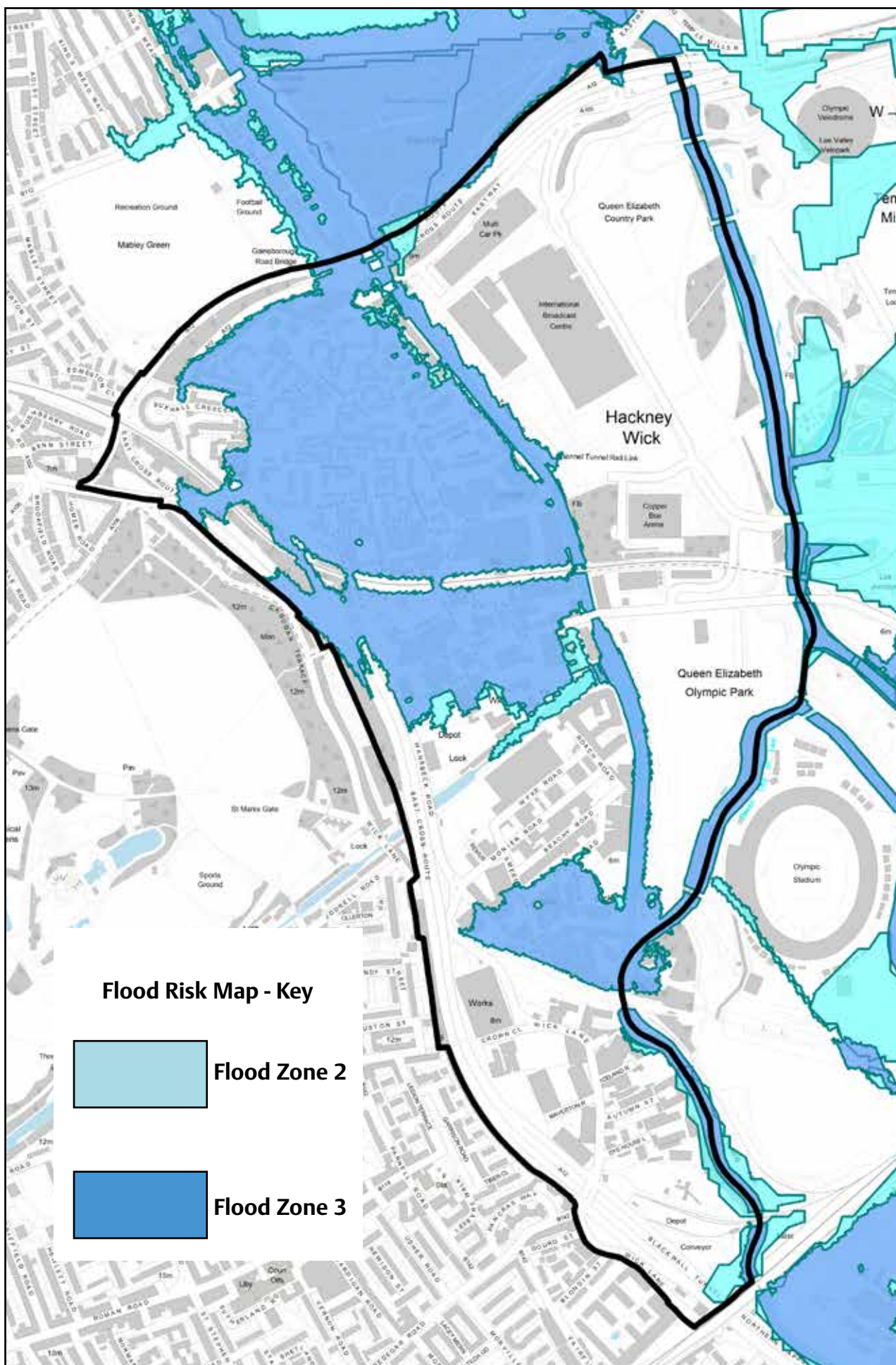
General Principles (all development)

- Include surface water run-off control measures, including Green Roofs, rainwater capture/harvesting, areas of green space and planting, permeable surfaces/paving.
- Attenuation measures within positive drainage to restrict surface water discharge rates to equivalent of greenfield run-off rates.
- Set back of development along water front areas.

Projects and Improvements:

Hackney Wick and Hackney Marshes Flood alleviation and habitat enhancement, including improved flood defences at Hackney Wick (medium to long-term project).





HERITAGE AND TOWNSCAPE

Strategy - heritage-led regeneration

A strategy of heritage-led regeneration is identified in the Local Plan for the core areas of change within Hackney Wick and Fish Island, integrating new mixed use development into the established street and waterway pattern and heritage assets that underlie the character that underlies the of the area, in particular that of the two conservation areas and their surroundings.

Key Local Plan Policies

- Policy BN.1 Responding to Place - responding to place, including respecting existing typologies in the urban fabric, including those of heritage value.
- Policy BN.10 Proposals for Tall Buildings - proposals need to preserve or enhance heritage assets and the views to/from these and positively contribute to their setting (including conservation areas).
- Policy BN.16 Conserving or enhancing heritage assets - a need to conserve or enhance heritage assets and their settings, including incorporation of viable uses consistent with their conservation and with heritage led regeneration.
- Policy 1.4 Preserving or enhancing heritage assets in Hackney Wick - preserve or enhance the special architectural or historic interest; enhance and reveal the significance of heritage assets; reference the architectural and historic interest within design of new development, retain or reprovide street trees where appropriate.

Conservation Areas

There are two conservation areas within Hackney Wick and Fish Island. Figure 3 shows the extent of each and the location of heritage assets. The conservation area appraisals and the management guidelines for each can be found on the Legacy Corporation [website](#).

The management guidelines contain specific guidance relevant to new development within or in the vicinity of the conservation areas which is not specifically repeated within the SPD.

Heritage Assets

Heritage assets are also identified within Figure 3. Any development proposal that is likely to have an effect on the identified heritage assets or their setting will need to be developed and assessed in accordance with the most up to date Historic England guidance 'Managing Significance in Decision-Taking in the

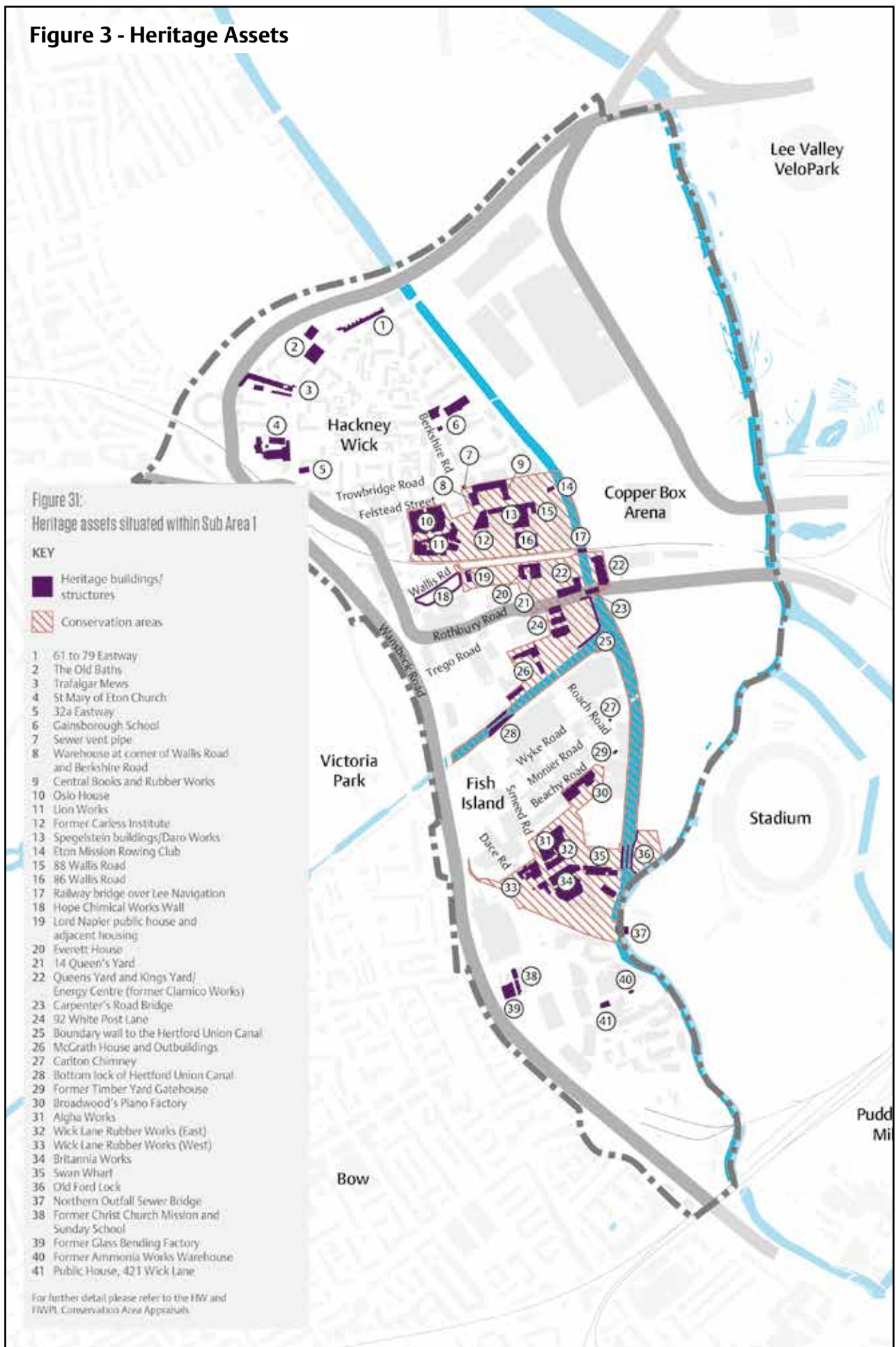
[Historic Environment - Historic Environment Good Practice Advice in Planning: 2 \(Historic England, March 2015\)](#) and this may include the need for a specific assessment of significance in accordance with this guidance.

Conservation Area and Listed Building Consent

It should be noted that the London Legacy Development Corporation is the determining authority for any application for Listed Building Consent or Conservation Area Consent within its administrative area.



Figure 3 - Heritage Assets



TOWNSCAPE AND CHARACTER

Built Form Massing and Grain

The built form of new buildings should respond to the distinct character and urban grain of the Conservation Areas. The area is characterised by large urban plots composed of multiple and amalgamated buildings and dramatic steps in height between buildings.

In proposed developments, longer blocks must be broken down through defined breaks or step in massing and form of at least 2 storeys. Development blocks should be arranged so as to create a courtyard or yard space within the block wherever possible.

Articulation in roof forms should be integral to the built form and express their function. Rooftop additions that are articulated as independent elements should be avoided as they are not consistent with the area's character. Elevations should have a predominantly consistent and simple roofline, and within the conservation areas should not compete with, or detract from, retained heritage assets.

Variety and Character

All buildings should consider orientation, with south facing elevations employing deeper reveals to provide solar shading and reduce overheating. Breaks in massing should be incorporated into



the southern side of blocks to maximize daylight penetration into residential courts and working yards, whilst also contribute to creating an appropriate character and urban grain. Setbacks on the courtyard or working yard facing side of blocks should be proposed to maximise the quality of daylight to residential units and courtyards.

New development must have well-defined frontage with the building line at the back of the footway. Within conservation areas, buildings should follow existing street corners. Corners must be strong and simple in form to create a vertically consistent and well defined frontage onto the public realm without balconies breaking the corner line. Any steps in height must be kept away from corners and ground floor building corners should have active frontages on both sides.

Careful consideration must be given to the transition between different street types within one building. Facades located on different typical conditions should have distinct elevational character whilst ensuring the whole building has a clear and legible identity. For mixed use buildings elevations the distinction between ground and upper level uses should be reflected in the design, scale of openings and overall character.

Responding to Streets

Buildings to primary streets should be designed to create a strong sense of definition with a composed and ordered character. Generally roof lines must be simple and well defined and the design of ground floors allowing for continuous active frontages.

Secondary street facing elevations have a more informal character reflecting the secondary nature of the network of smaller streets. Ground floor frontages within passages should be predominantly workspace uses informing a more solid ground floor and a strong integration between the built form and public realm.

Canal facing buildings should form an ensemble of buildings of varying scales along the canal edge. Elevations will be seen from a variety of scales and vistas, so should be characterised by large scale articulation, including groupings of floors and projecting balconies. Ground floors encourage a permeable and generous relationship between public uses and canal side

terrace spaces.

Facade Design

Elevation design and layout will need to balance good quality of daylight with appropriate privacy. It will be important that environmental mitigation be integral to the character and appearance of proposed buildings and not seen as additional elements that bring unnecessary complexity to the elevation.

Window reveals should generally be at least a full brick deep or flush with the facade to bring depth and definition to the elevation. Attention should be paid to the setting out of the facade elements to ensure an elegant and composed character. For buildings that can be viewed from a distance, the proposed scale and modulation of facade elements should take this into account; for example by creating larger apertures.

The careful selection and detailing of primary materials of proposed buildings should demonstrate how they positively contribute to the Hackney Wick and Fish Island Area. The detailing of fixings and interfaces between materials is equally important to create a sense of quality and avoid clutter. High quality brick and masonry should be the primary facing material for new development, although other materials can also be considered if there is a strong justification. Timber cladding, lightweight composite cladding systems and low quality materials should be avoided. Recycled, reclaimed and locally made materials should be used where possible.

Secondary elements such as windows, metalwork, balconies, signage and screens offer opportunities to introduce craft and refined ornament in the design of buildings, celebrating Hackney Wick's tradition as a place of creativity and making. Cast materials such as pre-cast concrete and ceramics are also encouraged where their manufacturing process contributes to a distinct identity. Carefully considered ornament, subtle detailing and clear architectural narrative should be integral to design of secondary elements within proposed buildings.

Adequate natural ventilation must be integrated into the design of windows/openings. The finish and colour of architectural metalwork should be selected to compliment the primary facing material and be considered alongside finishes of

windows and other elements of the facade.

Where users come into contact with the building, design should demonstrate increased durability, care and comfort of these elements and include softer, more tactile and robust materials. The design of signage should be considered at an early stage in the design and balance the need to be integrated into the building design whilst expressing the variety of uses.

Integrated Mix: Employment Use Design

The design of new workspace should be carefully considered in relation to the type of occupier envisioned whilst allowing sufficient adaptability to allow change over time. Any new workspace should demonstrate thorough consideration of appropriate floor to ceiling heights, layout, natural daylight and servicing for the specified workspace use. Commercial workspace should be designed to allow easy future sub division into smaller units.

At a neighbourhood block scale, a variety of workspace occupiers and typologies should be considered to contribute to the character of the area. Ground floor workspace frontage will need to balance active frontages and daylight with adequate privacy and practical internal arrangements. Workspace frontages to the public realm should incorporate raised window sills to create privacy and to allow for flexible planning of workspace interiors. Frontages to working yards must be more open and to allow opportunities for activities and uses to spill onto yard areas, and incorporate larger openings such as double height doors and roller shutters.

A Use Class Frontage should sit within the building framework set by structural columns and beams and should carry significant features through to ground floor level, rather than visually separating the ground and first floors, and enhance the character of the building.

The need for canopies, blinds and security shutters should be considered at an early stage when planning a new shopfront. Shutters should not be solid and laminated glass or internal shutters rather than external shutters are preferred. Commercial signage should be integrated into the design of architectural elements such as metalwork and joinery and contribute positively to the craftsmanship of the building.

[See Appendix 2 for more details](#)

PUBLIC REALM - MIXED USE AREAS

KEY DESIGN OBJECTIVES

New development in Hackney Wick and Fish Island should attempt to demonstrate they have addressed the following as part of any proposed public realm design.

1. Improve connectivity

Proposals should seek to increase the permeability of the area where appropriate; creating new passages and public spaces, and improve the surrounding street environment.

2. Be accessible, inclusive and safe

Proposals should promote accessible and inclusive design and comply with the LLDC Inclusive Design Standards, creating a lifetime area where all members of the community can live together throughout their lives with ease and in comfort.

3. Be simple, consistent, high quality and minimize clutter proposals should create a timeless and simple foreground to buildings that will age gracefully over time and create a sense of quality, reinforce the area's hierarchy of streets (including giving priority to pedestrian movement along primary routes) and seeking to reduce physical obstructions and visual clutter wherever possible.

4. Reflect the area's special qualities and distinctive character proposals should respond to the area's distinctive historic character in terms of streets, yards and passages, as well as the character of heritage assets and where possible reveal and reuse historic materials to create a positive and carefully considered setting to all identified heritage assets.

5. Be multi-purpose and provide amenity proposals should be designed for dual use and opportunities for seating and occupation of the public realm should be encouraged. Streets and public spaces should be a 'playable' landscape which will encourage children and their carers to use them. Larger scale sites in residential or mixed use areas will be expected to incorporate local Youth Play space provision.

6. Be sustainable Proposals should promote sustainable design including using materials that have a low carbon footprint, maximising on site sustainable urban drainage solutions and increasing biodiversity.

Streets & hard landscape

Proposed development provides an opportunity to enhance of existing streets and canal edges, but also create new routes and public spaces. It is important that the public realm provides a high quality, consistent, calm and uncluttered foreground that supports the activities of the local community, responds to the area's distinctive character, improves the pedestrian experience as the area becomes increasingly mixed use by creating a safe walkable area and reducing the domination of vehicles - whilst also enhancing the Conservation Areas and setting of heritage assets.

Footways must have a consistent approach to finish and colour, that are appropriate to their context and must give priority to pedestrians, including at vehicle crossovers.

Hard landscape materials with busy patterned surfaces such as stripes that cause confusion or are disorientation in particular to people with visual impairments should be avoided, and changes of material should be used as opportunities for demarcating space, for example extending a tree pit surface to create a space to sit around a tree.

A hard landscape materials site audit should be completed before commencing detailed design, in order to identify opportunities to recycle and reuse on site materials and uncover historic materials and features that could be re-cast or re-laid.

On sites that tie into key primary or secondary routes, the public realm must directly continue through the development as traditional streets providing primary access for vehicles, pedestrians and cyclists.

On primary and wider secondary streets proposals must include provision for inset on street parking and loading bays, including visitor blue badge parking bays.

Soft landscape & play

The soft landscape should be fully integrated into to any new development and considered as 'Green Infrastructure' and a multi-functional working landscape. It should not be decorative, but should be part of place-making, climate change mitigation and an integral part of how the place functions.

New development should consider how the landscape can contribute to water management through, for example SuDs and green roofs, urban heat island effect through shading and biodiversity .

An approach to landscape and planting typologies should include streets, squares and pocket parks, as well as opportunities around the water edge, walls and roofs. New street trees should be provided on primary and secondary streets

Green infrastructure can be defined as (GLA):
A network of green spaces - and features such as street trees and green roofs - that is planned, designed and managed to provide a range of benefits, including: recreation and amenity, healthy living, reducing flooding, improving air quality, cooling the urban environment, encouraging walking and cycling, and enhancing biodiversity and ecological resilience.

Planting and playspaces will need to be fit for purpose, robust and adequately specified to ensure longevity. In particular its ability to 'add value' whether through character, local provenance, ability to encourage biodiversity or other functionality should be encouraged. Its vital that its long term management and maintenance is considered in parallel with its design. Where appropriate expert ecological or horticultural advice must be demonstrated as part of the process such as where green roofs, walls or SuDs are proposed.

Setting of buildings, steps & ramps

Particular attention should be paid to the setting of historic assets and streets within the conservation area where the interface between existing and new buildings and the new public realm surfaces should be formed by distinct and consistent edging details.

New steps, ramps and retaining structures must be designed to be simple and robust and reduce the risk of vandalism and maintenance costs, with a consistent approach to finish and colour, that is appropriate to context

New steps, ramps and retaining structures should be designed to also serve as places to meet, sit and be used as spaces of informal play where level changes occur to provide opportunities for dual functions such as steps doubling as seating elements or opportunities for play whilst minimising street clutter.

Street furniture & lighting

Street furniture must have a consistent approach to finish and colour, that are appropriate to their context

A street furniture audit should be undertaken before commencing detailed design to determine what existing street furniture and lighting is in place, and what should be removed.

Street furniture must be of robust construction,

resistant to vandalism and be placed in positions that minimise risk of damage by vehicles and clustered into appropriate zones such as where the footway widens and opens into key public spaces to avoid physical obstructions and respect pedestrian desire lines.

New built in street furniture should use a colour palette and texture palette that reflects the materials of existing buildings, including yellow stock brick and purple engineering brick colourings and in situ concrete finishes.

Use of bollards should be minimised and all freestanding metal street furniture should be powder coated black.

New lighting should be consistent and lux levels appropriate to use, including avoiding pooling of light and shade to aid people with visual impairment and assist in the night time protection of property; discourage crime and vandalism; make residents and street users feel secure and enhance the appearance of the area after dark.

Street lighting should be positioned at back of pavement on primary streets and building mounted on passages and courts.

Signage & creating an uncluttered environment

New signage must ensure sight lines, lighting, visual contrast and legibility are appropriate.

Building planning layout must consider the logical layout of facilities and the need to support ease of wayfinding. Street layouts, geometries and networks should aim to make the environment self-explanatory to all users.

Adjacent developments should work to develop a comprehensive approach to signage to ensure consistency and avoid duplication. All signage should be simple and consistent.

Signage should be dual purpose and consolidated where possible, for example signage mounted on lampposts or combined. The location and design of signs and signposts should be planned to permit effective maintenance (including access for cleaning equipment) and to minimise clutter.

Legible London signage should be installed on key spaces and on key pedestrian and cycle routes.

Within the conservation area opportunities for reinstating signage on heritage buildings, uncovering historic murals and opportunities for interpreting of the area's heritage should be maximised.

[See Appendix 2 for more details](#)

2. AREA SPECIFIC GUIDANCE

NEIGHBOURHOOD CENTRE

Character

Established around its historic street pattern, the Neighbourhood Centre varies in its existing form from modern, mixed use residential and employment buildings and employment and retail buildings at Oslo House and Lion Works, the employment and workspace character of the streets and yards continues eastward along Wallis Road and White Post Lane.

To the east of the River Lea Navigation, the Copper Box provides a major modern leisure venue and setting, with land to the north and west awaiting development for a mix of town centre uses and new homes that compliment the Neighbourhood Centres functions (part of the Legacy Communities Scheme). To the south of the Kings Yard Energy Centre, the centre includes the location of a library/Ideas Store and medical facility that will form part of the Legacy Communities Scheme Sweetwater area.

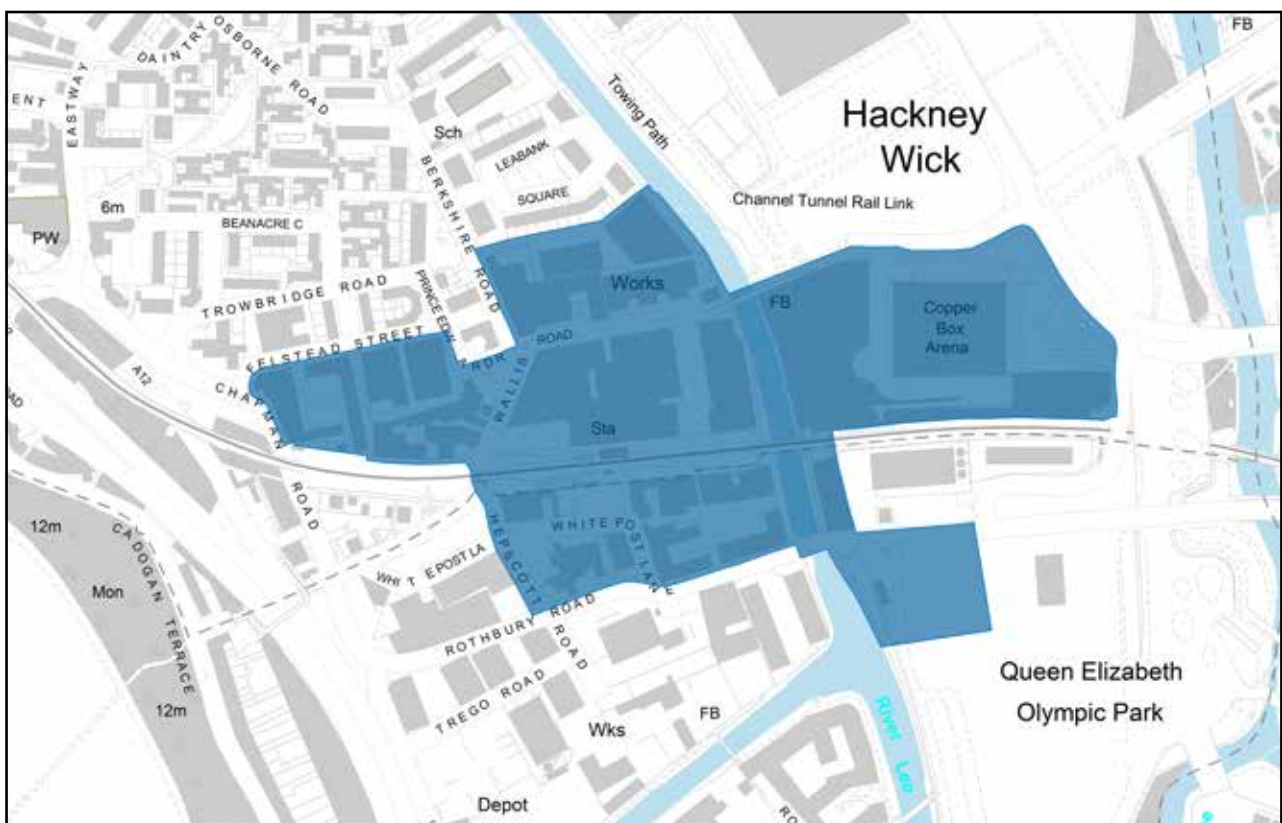
The existing and emerging character are that of generally moderate height buildings and a variety of

scales and uses that establish the core functions of the new Neighbourhood Centre which will see comprehensive but sensitive change in its core around Hackney Wick Station.

Strategy

The Local Plan designates the extent of the Neighbourhood Centre, with the intention that the existing element of the centre in the west around Felstead Street is protected and the planned mix of retail and employment uses within the Legacy Communities Scheme to the north and west of the Copper Box are delivered, providing a link between East Wick and Here East and the rest of the centre.

Likewise, the delivery of the community facilities, including medical centre, library/Ideas Store and nursery immediately to the south of the Kings Yard Energy Centre will tie the new Sweetwater Neighbourhood into the centre and link to the station. The new station underpass, as part of the new north-south pedestrian/cycle link will bring the two halves north and south of the railway line, together, with this and the wider improvements to Hackney Wick Station providing a catalyst to comprehensive regeneration.

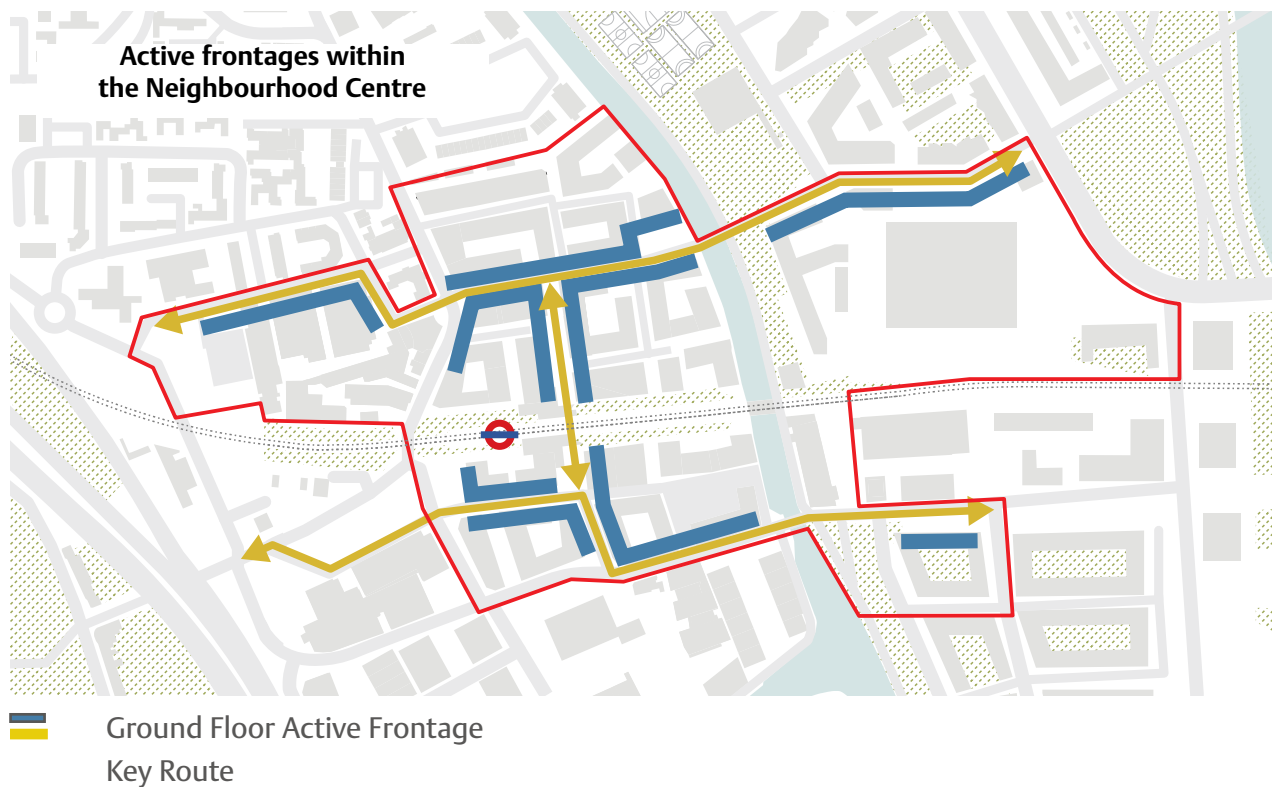


Achieving the strategy - principle elements

The following sets out the principles that will help development achieve a coherent and comprehensive approach to developing the neighbourhood centre.

Active Frontages, retail and community uses

Retail, food and drink, along with community, leisure and office uses are acceptable within the Neighbourhood Centre boundary. Retail, leisure and community uses should generally be focused along the key active frontages where they are most visible and accessible.

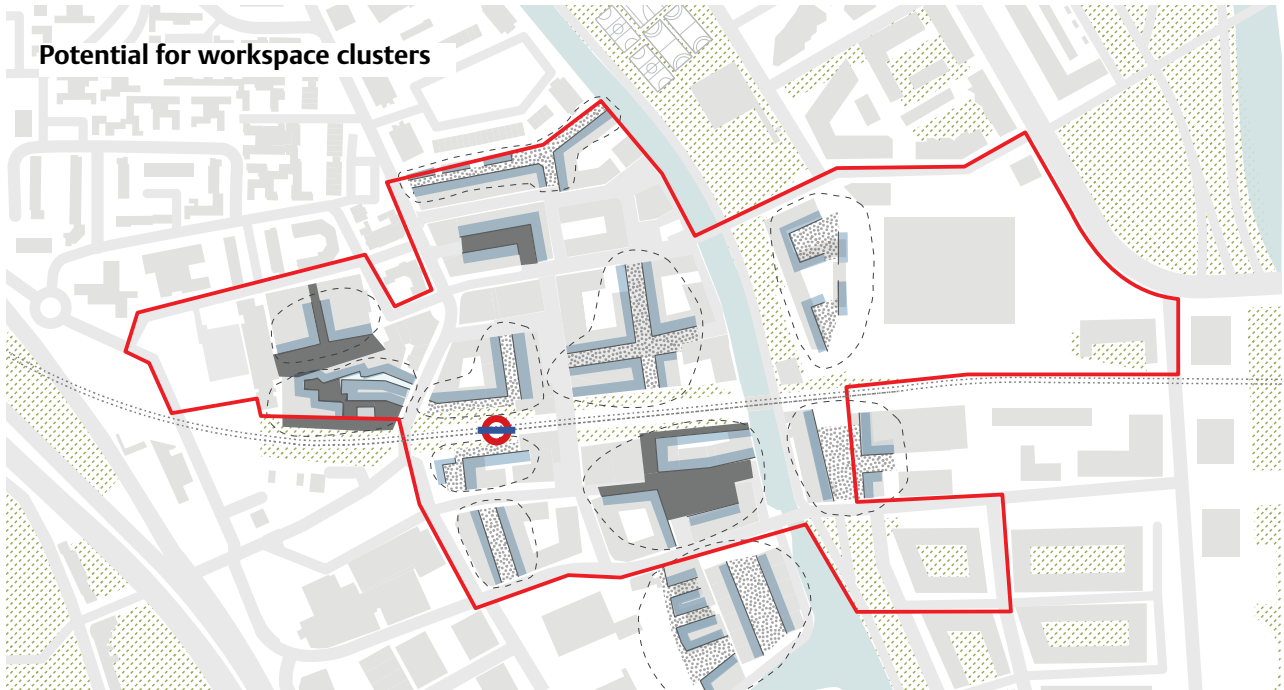


Clusters of workspace

Potential to achieve clusters of workspace across the centre focusing on Use Class B1a and B1c formats compatible with the wider mix of centre uses, including residential. Changes from B2 and B8 employment uses must meet the tests in Local Plan Policy B1.



Potential for workspace clusters



- Historic Working Yard
- Indicative Working Yard

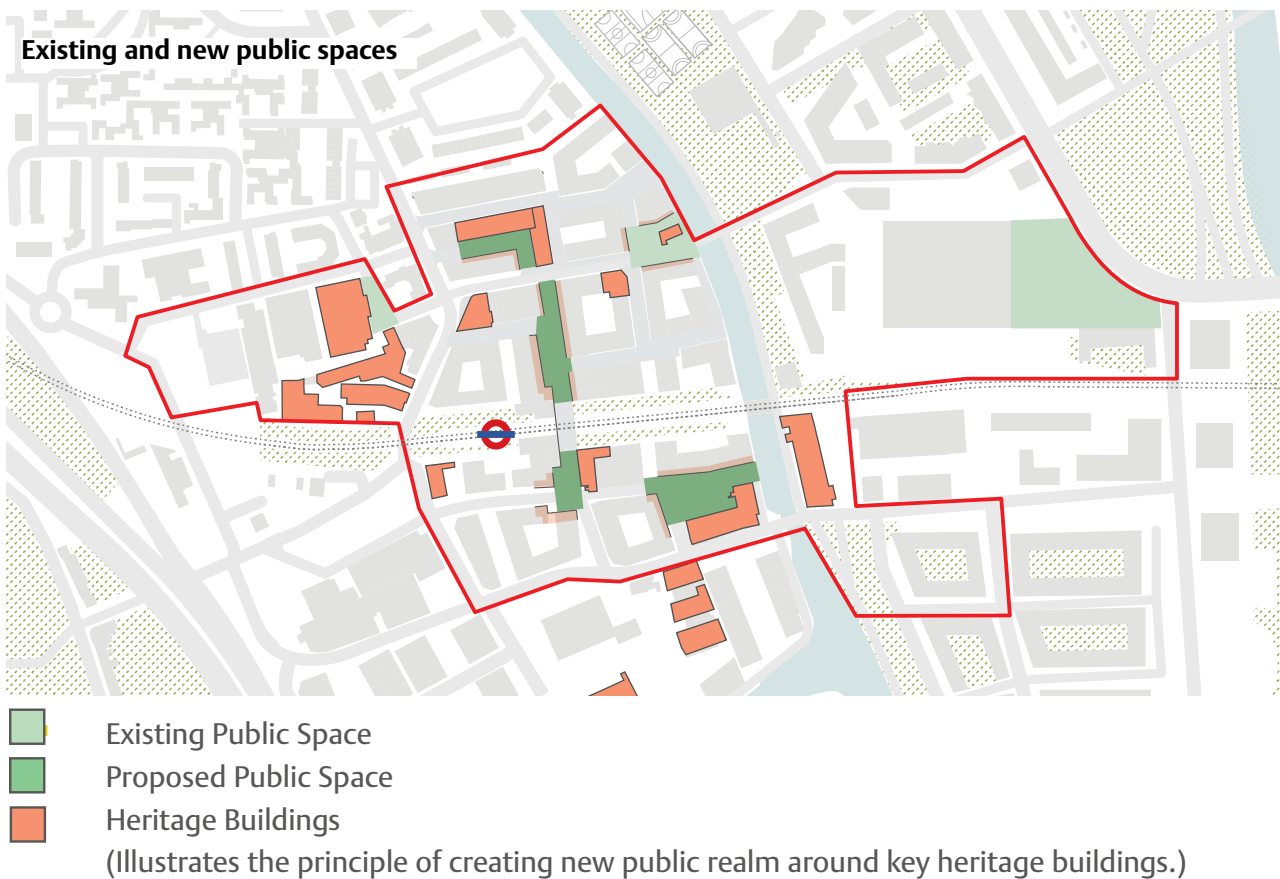
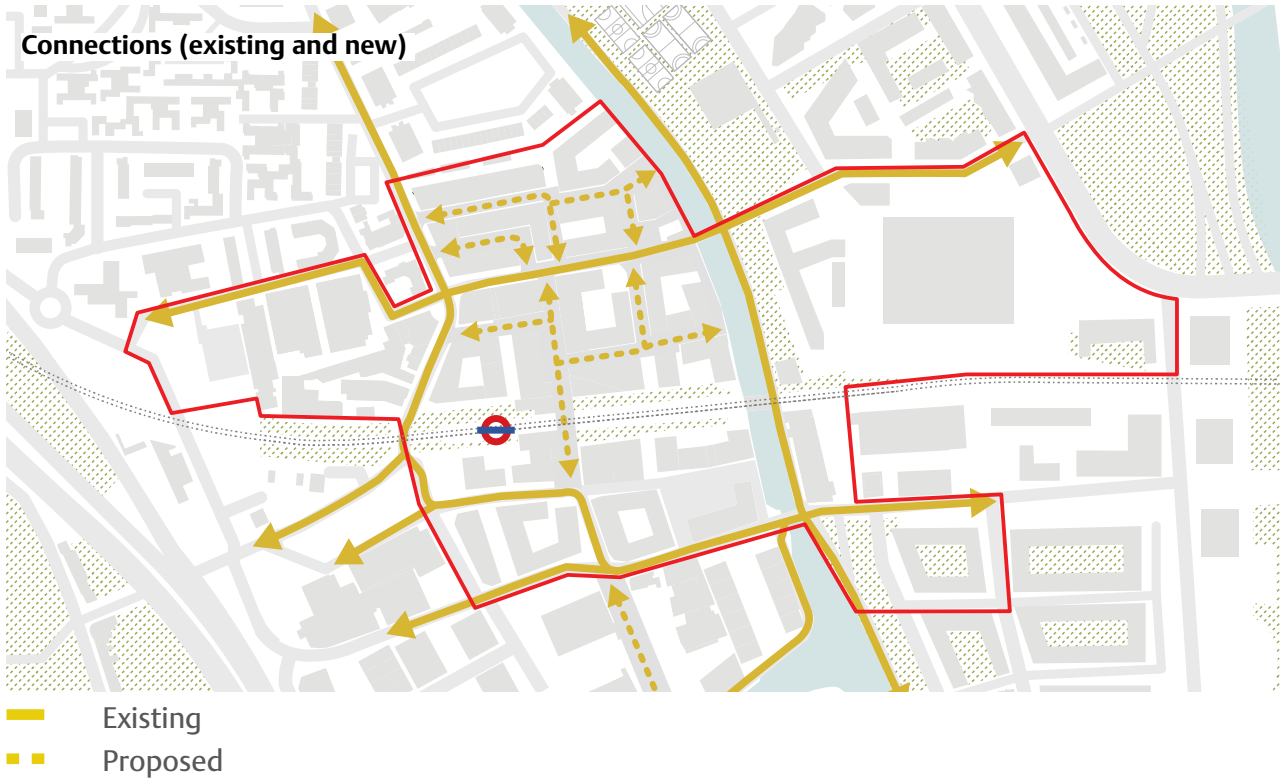


Connections

The centre will be defined and tied together through development focused around existing and new connections (see Projects and Improvements).

New public spaces

A series of new multi-purpose public spaces that help to anchor activity and movement in the neighbourhood centre

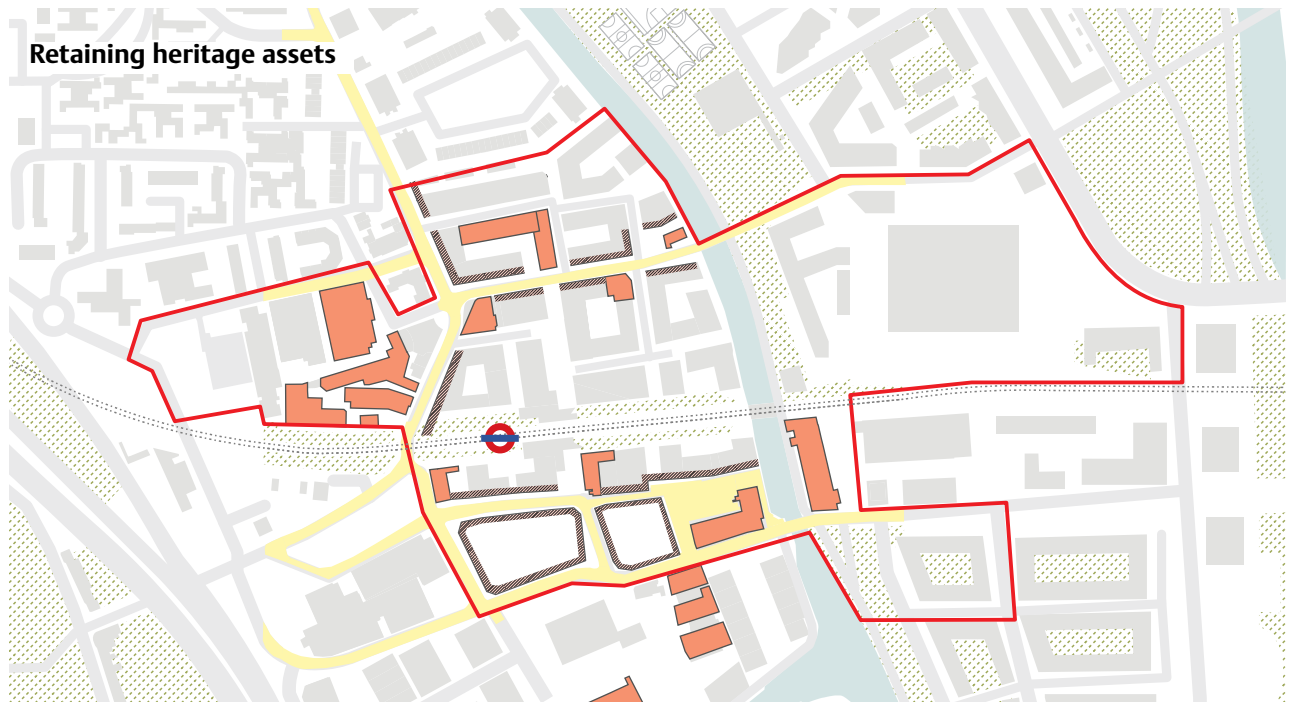


Conservation Area and Heritage Assets

Retained heritage assets and historic street pattern ensuring the character and scale of the neighbourhood centre reflects its past.

Achieving a comprehensive approach

Coordination and cooperation to ensure individual sites deliver a coherent outcome and incorporate key spaces and connections.



- Heritage Buildings
- Historic Streets



Projects and Improvements

Hackney Wick Station - new station underpass (part of new north-south route) and station improvement works

Establishing north-south route to the north and south of Hackney Wick Station

Cycle access improvements to Bridge H10 at Wallis Road

Improved canal walls along Lee Navigation to reinforce flood defences

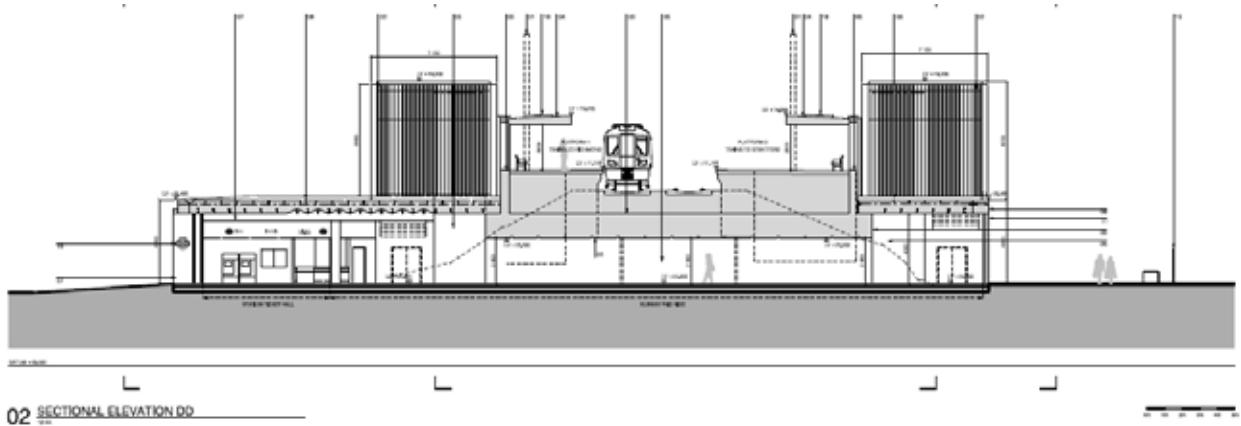
District heating network installation

Improvement/restoration works to heritage assets where required



North-South underpass at Hackney Wick Station.

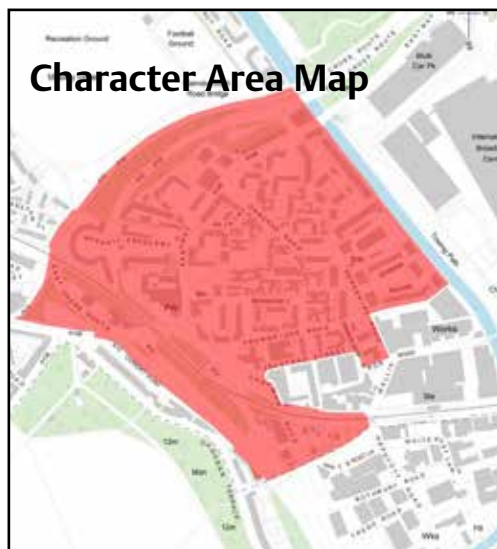
This will form a key component of the new North-South Route and is a key component of the Hackney Wick Station upgrade works due to be implemented during 2016/17. The planning permission for these works provide greater detail [Planning application reference 14/00275/FUL].



EASTWAY, TROWBRIDGE ESTATE AND WICK VILLAGE

Character

Low density mixed use area with a core of low level predominately family housing surrounded by an arc of employment sites, community uses and local retail interspersed with some housing. The Trowbridge Estate, Wick Village and Leabank Square provide the core of quite residential streets with community uses focused along the Eastway and at Gainsborough Community Primary School. The A12 to the north and west and the River Lea Navigation/Hackney Cut to the East act as boundaries and barriers to local movement. Eastway provides the main through route, providing connection to the rest of Hackney to the West and Queen Elizabeth Olympic Park and Here East to the East.



Issues and Opportunities

- A12 as a barrier to local movement and source of noise and pollution.
- Lea Navigation/Hackney Cut as barrier to local movement and environmental resource.
- Railway as barrier to local movement and source of noise.
- Interface with the existing uses and new development within the Neighbourhood Centre.
- Employment sites and buildings as resource for existing and future local businesses and employment opportunities.

Strategy

Reinforcing the continuity of an established neighbourhood of family homes and local businesses, ensuring that the new development in the Neighbourhood Centre respects the scale and residential use of the area it adjoins and provides a

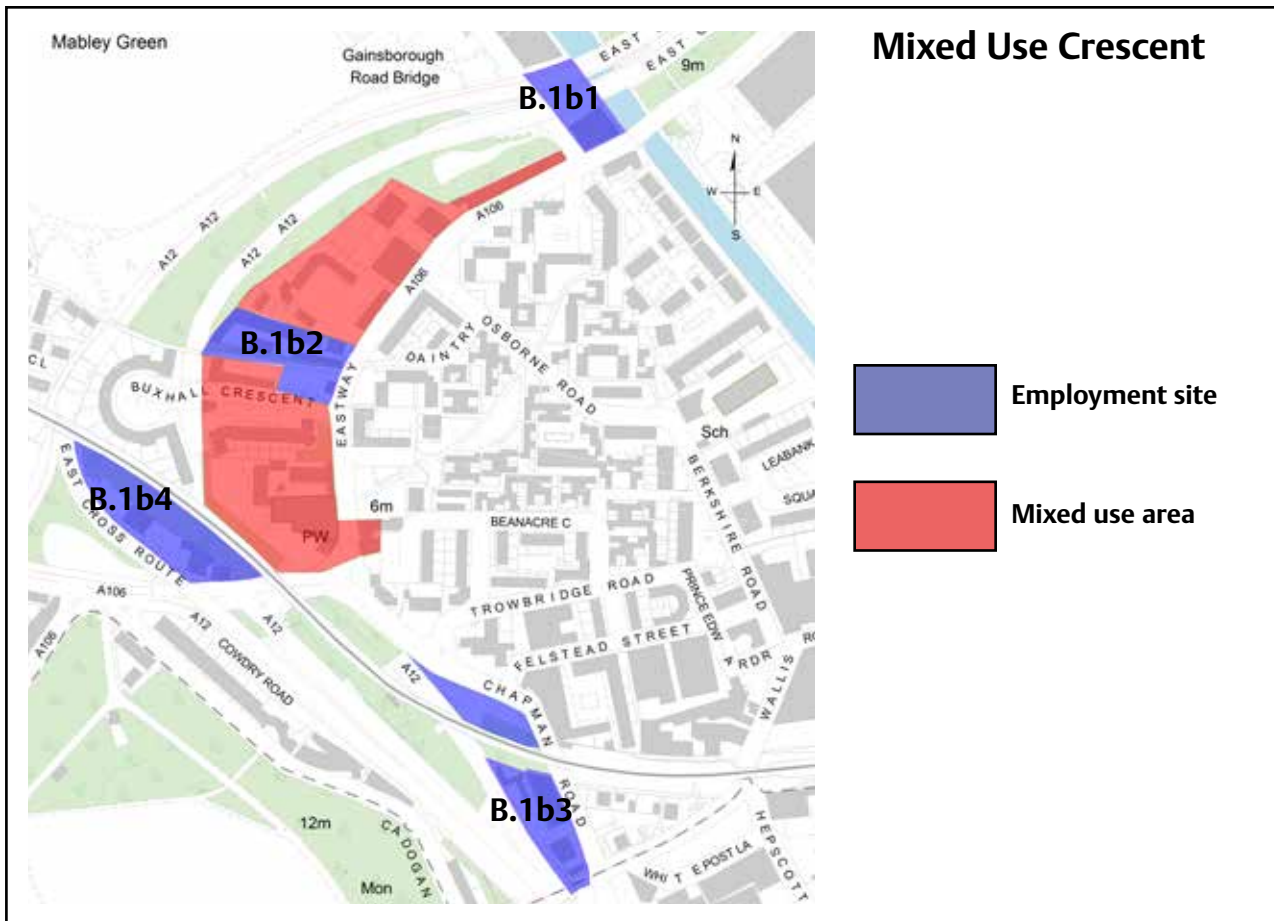
safe and accessible public realm at this interface. Taking opportunities to improve local connections and routes where opportunities to do so arise. Maintaining, and where needed, improving green spaces, play areas and equipment, street furniture and the wider public realm.

Sites and uses

No significant change is envisaged within the Local Plans strategy for this location. The following sites are identified as protected employment sites or site allocations within the Local Plan:

Site name/ description	Description
Site at junction of Lee Conservancy Road and Eastway - Locally Significant Industrial Site (ref: B.1b1)	B Class industrial use and suitable for future industrial, storage and distribution or transport related use (Cluster function description from the Local Plan 2015)
Trafalgar Mews, Eastway - Locally Significant Industrial Site (ref B.1b2)	Mixed industrial and business use and transport related use (Cluster function description from the Local Plan 2015)
Site at Chapman Road - Locally Significant Industrial Site (ref: B.1b3)	A mix of small scale industrial, storage and distribution uses. om the Local Plan 2015)
Bartrip Street North - Locally Significant Industrial Site (ref: B.1b4)	A mix of small scale industrial, storage and distribution uses. om the Local Plan 2015)
Bartrip Street South (Local Plan Site Allocation SA1.9)	Local Plan 2015 allocated use: Land with potential for gypsy and traveller site.





FISH ISLAND MID

Character

Fish Island Mid, between the Neighbourhood Centre in the north, Hertford Union Canal to the east, the Greenway to the south and A12 to the west, comprises a wide mixture of sites in a range of employment uses, set within a fine scale road network, and mixture of narrow urban blocks, typical of the area as a whole, along with several larger sites. The Fish Island and White Post Lane Conservation Area delineates a concentrations of heritage assets and the historic interest of the canals in shaping the areas' form and character.

Omega Works represents the only current concentration of residential use. Existing built form is low to medium density in character with a variety of physical condition and built quality. The imminent development of the Neptune Wharf and adjacent sites between Wyke and Monier Roads, are due to change this balance and character significantly.



Issues and Opportunities

Availability of sites for new development

- character of the existing road network
- significant lengths of waterway site frontage
- Characteristics and built form within and adjacent to the conservation area
- diversity of existing
- business mix within this area
- Poor quality of the public realm
- Adjacency of A12 (noise and pollution) along

western edge of the area

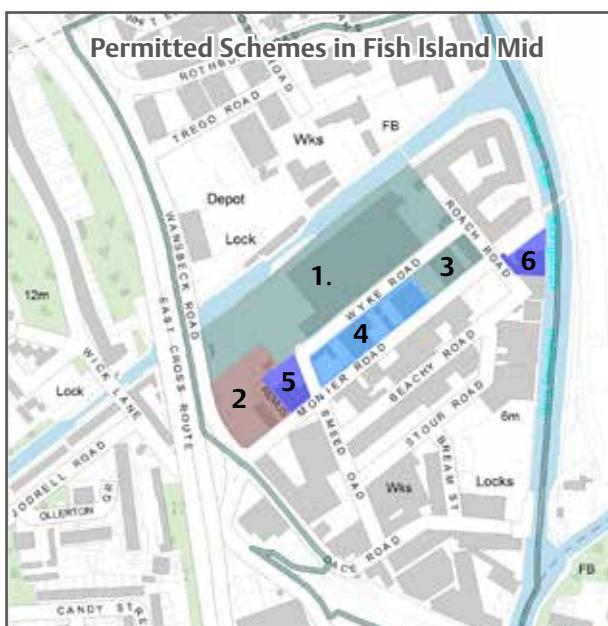
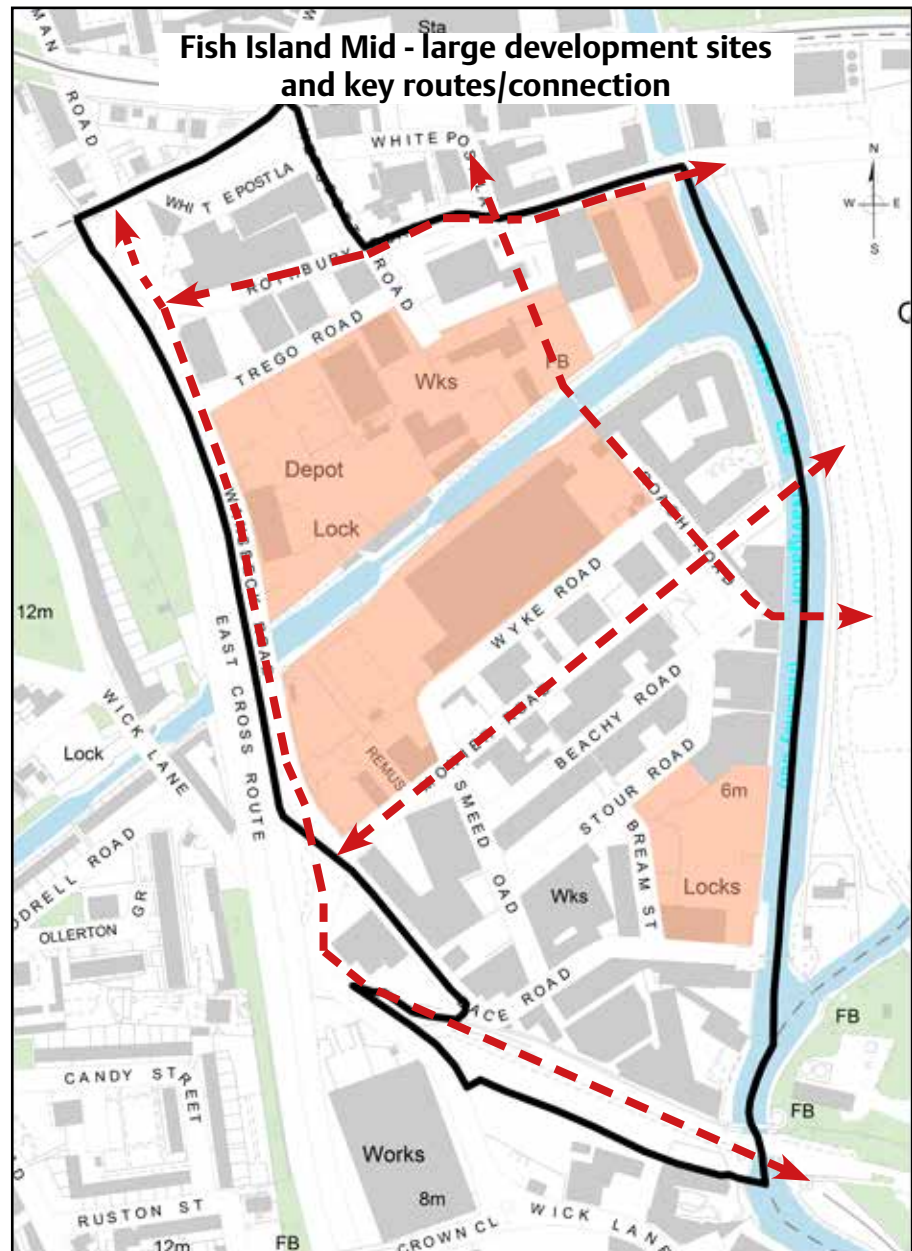
- Opportunities to active the waterways and waterway frontages.
- Completion of the North-South Route to increase accessibility between Hackney Wick and Fish Island Mid and South.

Strategy

With all sites in private ownership, delivery of major mixed use development within the site allocation areas, while outside of the site allocations and conservation area, smaller scale mixed use development replacing existing employment floorspace and providing new residential development in accordance with Local Plan policies and in the context of the Fish Island Conservation Area. Securing delivery of the north-south link route, the all-modes Monier Road Bridge to Sweetwater, general public realm and canal frontage improvements and delivery of the Neptune Wharf Primary School.

Sites and uses

Site name/ description	Description
SA1.2 Hamlet Industrial Estate	Mixed-use development including employment and residential floorspace complimented by restaurants/cafes
SA1.3 Hepscott Road	Comprehensive mixed-use development including employment, residential, creative and cultural uses and a linear park
SA1.4 Bream Street	Mixed-use development including employment, residential, and creative and cultural uses, to come forward in a comprehensive manner.
SA1.6 Neptune Wharf	Comprehensive, phased, mixed-use medium density residential development, incorporating public open space and land for a future primary school to help meet education needs arising across Fish Island.



Permitted Schemes in Fish Island Mid		
Map No.	Application Reference	Scheme
1.	12/00210/OUT	Neptune Wharf
2.	12/00210/OUT	Neptune Wharf Primary School
3.	13/00204/FUM	Monier Road East
4.	14/00374/FUL	Monier Road West
5.	15/00212/FUL	33-35 Monier Road
6.	14/00260/FUL	4 Roach Road

Conservation area and heritage assets

The Fish Island and White Post Lane Conservation Area and the heritage identifies three clusters of heritage assets:

- Dace Road Cluster
- Stour Road Cluster
- White Post Lane Cluster
-

Along with the canals and associated historic infrastructure, these comprise the core of the conservation area. The focus will be the retention and continued use/re-use of heritage assets with the form, height, materials and wider design of any surrounding development conserving and enhancing the heritage assets and their setting, retaining and enhancing the character of the area.



Approved schemes - Monier Rd/Wyke Road



Projects and Improvements

Monier Road Bridge Upgrade

North-South route (including Hertford Union canal bridge replacement)

General highways and public realm Improvements

District Heat Network installation/extension (including potential canal crossings)

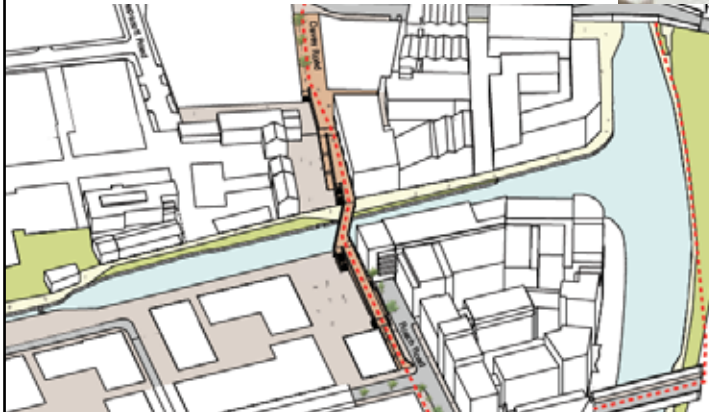
Neptune Wharf Primary School



Neptune Wharf School visulisation

Hertford Union Canal Bridge replacement.

Replacement of existing bride with wider more accessible bridge to form an integral part of the new North-South Route.



Bridge H14 (Monier Road Bridge) replacement.

Planned replacement of temporary walking and cycling route bridge to provide a road bridge access as part of the Legacy Communities Scheme. A requirement of the Legacy Communities Scheme planning permission.



Bridge H16 (Beechy Road) new pedestrian and cycle bridge to Sweetwater.

Planned new pedestrian and cycle bridge to provide link between Beechy Road and the planned Legacy Communities Scheme Sweetwater development. A requirement of the Legacy Communities Scheme planning permission.



FISH ISLAND SOUTH

Character

An area of predominantly industrial and transport related uses mostly to the south of the Greenway. The majority of the area is designated as Strategic Industrial Land (preferred Industrial Location). It includes a range types and scales of use from rail bournie aggregate delivery and concrete production at the London Concrete premises, to smaller scale industrial and business park style units. One or two historic individual residential properties also exist further south on Wick Lane.

The northern part of the area is designated as an “Other Industrial Location” (OIL) which continues to have a focus on industrial, storage and distribution and other employment uses but also includes some more recent purpose built live/work development. Some live work units have become purely residential in use but the overall focus of the OIL area continues to be for a range of sizes of industrial, storage and distribution uses.

Issues and Opportunities
The strategic importance in London and Legacy Corporation area of Strategic Industrial Land and opportunity to intensify and grow the cluster of employment uses.
Easy access to A12 providing access to wide range of markets, ease of delivery and servicing for industrial uses.
Environmental quality related to the industrial uses and proximity of the A12 and railway corridor.
Poor quality of buildings and yard areas in places.
Constrained road access and connectivity along the smaller side streets.
High accessibility to the strategic road network (A12 and Blackwall Tunnel)
Rail access within southern part of the area.
Potential for intensification of industrial and other employment (Use Class B2, B8 and B1(c)) uses within parts of the area.
Opportunity for limited introduction of residential development within the Other Industrial Location area, where compatible with the industrial and other employment uses.
Opportunities to improve access to sites and improve environment along Wick Lane.

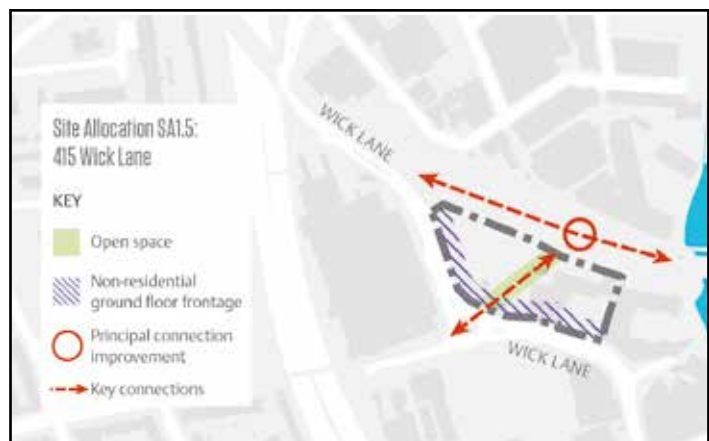
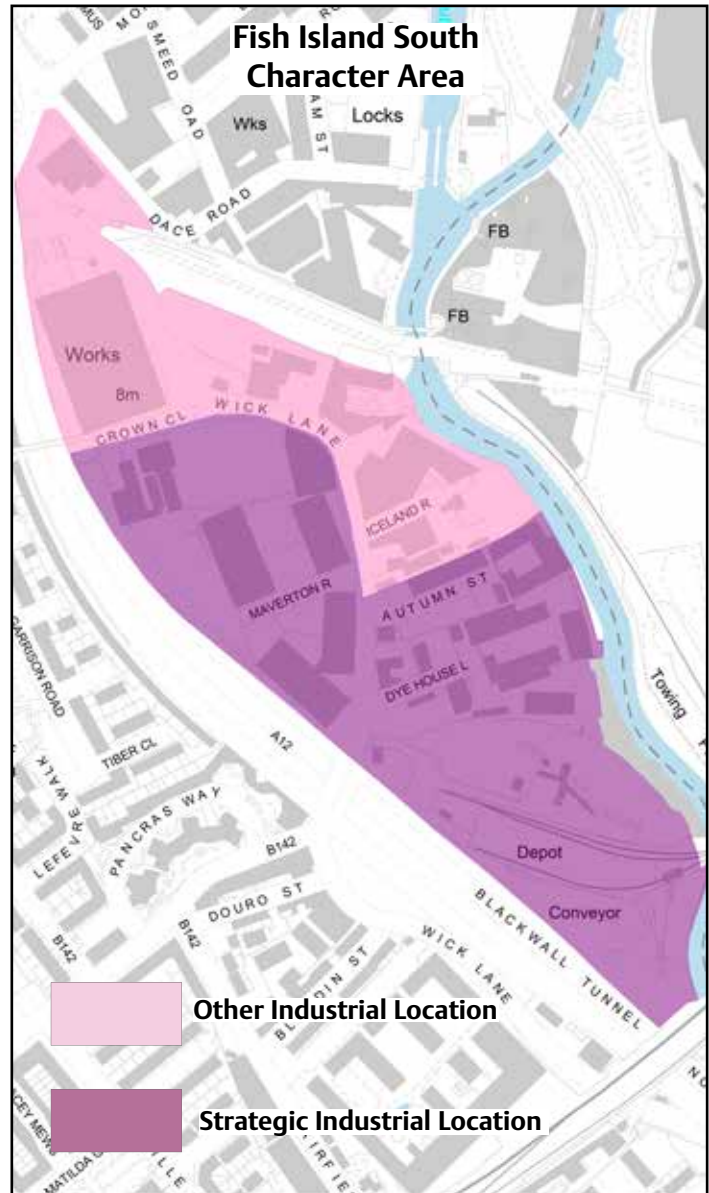
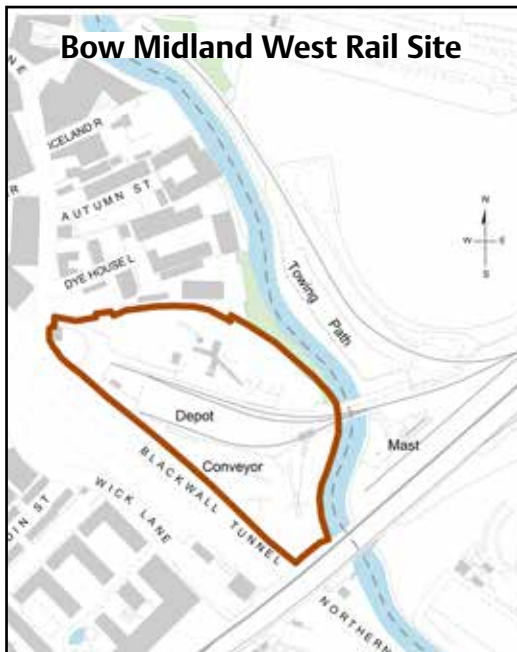
Strategy

Maintain, protect and where possible intensify Strategic Industrial Land uses within the allocated SIL area and avoid introduction of sensitive uses close to the SIL boundary within the area surrounding it. Avoiding residential use on SIL boundary. Maintain an industrial use relationship between Fish Island South and Bow East to the eastern bank of the River Lea. Protect and intensify employment uses within the Other Industrial Location and manage the introduction of a small element of residential development where this will remain compatible with the existing or new employment uses, and provides a transition in uses and environmental quality between the SIL area and Fish Island Mid to the north. Take account of the value and character of the identified Heritage Asset on Crown Close and Iceland Road.

Sites and uses

The following are the key policy designations and sites within Fish Island South:

Site name/ description	Description
Wick Lane & Crown Close Other Industrial Location (OIL) Local Plan ref: B.1b5	An employment led mix of uses, including warehouse, storage, distribution, with some potential for residential development and live work in appropriate locations, subject to Policy B.1 (5). AN appropriate and gradual transition between nearby uses of residential and industrial.
Fish Island South Strategic Industrial Land (Preferred Industrial Location) . Local Plan ref: B.1a2	A range of significant B2 and B8 Use Classes of industrial, warehousing, transport, waste management and distribution.
Bow Midland West Rail Site (part of Strategic Industrial location) Local Plan ref: B.1a2	Safeguarded rail head, including potential for aggregate distribution and for concrete batching, the manufacture of coated materials, other concrete products and handling, processing and distribution of aggregate material.
SA1.5 Wick Lane site allocation	Mixed-use development, including employment and residential, to provide transition between the Fish Island SIL and mixed use character north of the Greenway.



Projects and Improvements

Ramp/stairs to Greenway with link through 415 Wick Lane site

A12 Crown Close to Roman Road Link - replacement of pedestrian and cycle bridge to provide all-modes link.

General highways and public realm Improvements

Greenway connection to Wick Lane

New direct connection required in Local Plan site allocation SA1.5 '415 Wick Lane' as part of a development scheme on that site. Potential zone for the connection shown below.



Old Ford A12 Bridge Replacement

Long-term project for potential replacement of existing pedestrian and cycle bridge with a new road bridge across the A12 connecting Fish Island and Bow.





THE LEGACY DEVELOPMENT AREA

Character

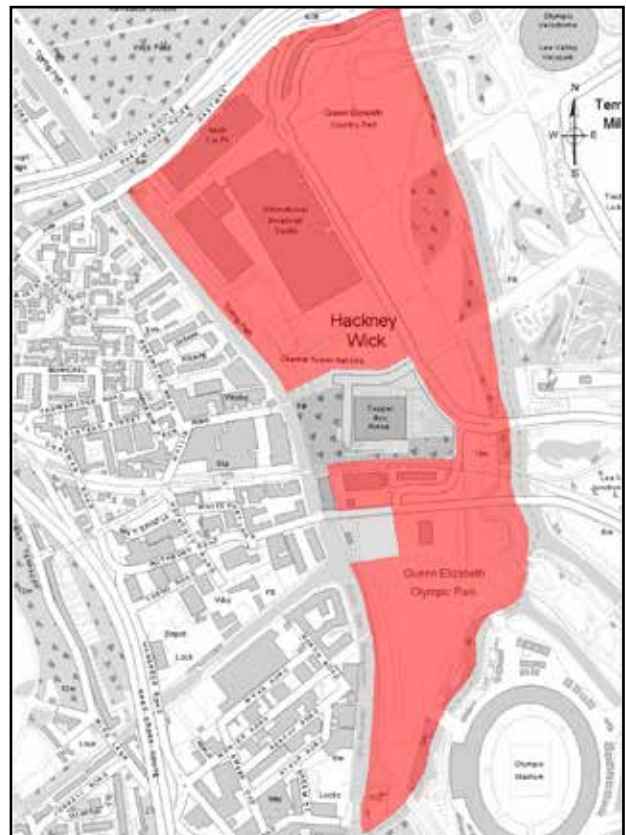
Surrounded on all except its northern edge by waterways and on that edge by the A12, this Legacy Development area is connected east and west by a series of all mode and foot/cycle bridges. The parklands to the east and the established but changing areas of Hackney Wick and Flsh Island to the west provide a contrast of character along each edge and the opportunity to create a sustainable and modern interpretation of the traditional London neighbourhood within it.

Strategy - implementation of the Legacy Communities Scheme and Here East

With implementation of Here East well underway and the Eastwick Primary School complex due to open in September 2016, the strategy in place is to continue delivery of development at East Wick and Sweetwater in accordance with the approved outline Legacy Communities Scheme planning permission. Further details are being brought forward through zonal masterplans and detailed planning applications.

The main components of the development

Here East: a major technology focused employment hub incorporating research and development, education and broadcasting, providing opportunities for a range of businesses types and sizes. Elements are already operational with conversion and fit out through 2015 and 2016.



East Wick:

- Up to 9,000 sqm of flexible commercial spaces (B19(a), B1(b), B1(c))
- Up to 4,725 sqm of retail (A1-A5) and leisure (D2) uses
- Up to 6,888 sqm of community (D1) floorspace, including a new primary school and nursery.
- Approximately 900 new homes (mix of town houses, terraced houses, mews houses and flats).

Construction of the school began in 2015, with first phases of the main development due to start in late 2016.

Sweetwater

- Approximately 650 homes
- Up to 1,065 sqm of B1(a) employment space
- Up to 2,576 sqm of retail (A1-A5) space
- A new primary school, playingfield, health centre, library and nursery.

Start of development is planned for 2016/17.

Relationship to the Lee Navigation and the West

This is the most important developing relationship with the Canal Park and towpath along the east of the Lee Navigation providing an accessible and more open frontage for this area's western facing development, while the western bank provides a new mixed, harder edged approach with gaps provided by breaks between buildings (existing and future) and by occasional areas of public realm. The series of all mode and pedestrian/cycle bridges provide the connections that will tie the new Here East, Eastwick and Sweetwater communities to the more established by developing ones in the rest of Hackney Wick and Fish Island.

Relationship to Parklands to the east

The Queen Elizabeth Olympic Park parklands provide a unique eastern aspect to the area, in particular East Wick, providing both amenity and leisure as well as a visual and environmental quality aspect that differentiates the location from the new parts of Stratford towards which it looks.

Phasing and interim uses

The significant amounts of development to take place in Eastwick and Sweetwater will necessitate phasing of delivery providing the opportunity for interim uses to maximise the activity in the short term. In introducing any interim uses the requirements of Local Plan Policy B.3 (Creating vitality through interim uses). This emphasises the need to

ensure that interim uses are compatible with the surrounding permanent, including planned, development and preferably help to establish the long-term character and types of uses for that location.

Projects and Improvements
Monier Road Bridge Upgrade - change to all modes bridge
Bridge H16: new pedestrian and cycle bridge to Sweetwater
Sweetwater Primary School
Sweetwater Library/Ideas Store
Sweetwater Health Centre
Sweetwater school playingfield and open space
East Wick Primary School, including playing pitches (shared with Gainsborough Primary School)
Canal Park (along East Wick and Sweetwater Lee Navigation Frontages)
Potential heat network connections to 'off park' locations utilising new and existing bridges.

3. IMPLEMENTATION AND DELIVERY

Change in Hackney Wick and Fish Island will not by its nature be a single project or even a series of coordinated projects. Rather, change will be a combination of specific regeneration projects that result in new development, in particular the East Wick, Sweetwater and Hackney Wick Central schemes that are led by the Legacy Corporation directly and a range of other independent development schemes that are predominately delivered in response to market demand. The key element to ensuring the success of these elements in creating successful places, aside from successful, high quality design, will be the delivery of key elements of physical and social infrastructure. In most cases, where change is outside of the direct control of the Legacy Corporation, specific intervention to make development happen will not be necessary. However, where it becomes necessary to assemble viable sites or acquire land to ensure delivery of necessary elements of new physical infrastructure, the use of the Development Corporations compulsory purchase powers will remain as an option.

This section of the SPD outlines the overall delivery options and approach that will be taken by the Legacy Corporation to achieve its aims in Hackney Wick and Fish Island both as a regeneration body and as the Local Planning Authority.

DIRECT INTERVENTION

The delivery of the planned development at East Wick and Sweetwater has been pursued by procurement of a development partner to deliver the homes, business and community space and other development elements that have been permitted within the Legacy Communities Scheme planning permission. Direct delivery will therefore be responsible for a significant element of the change that is envisaged in this SPD.

PARTNERSHIP WITH OTHERS

The boroughs (LB Hackney and LB Tower Hamlets) are key partners in the change planned for Hackney Wick and Fish Island, with their role as provider of a range of local services and in particular as highways authorities. The canals and River Trust and Legacy Corporation, through their joint Waterways Strategy,

have established a coordinated approach to management and developments in the use of the network of waterways in this area. Lea Valley Regional Park Authority as a landowner in the north of the area and owner and operator of the Lea Valley Velopark which extends into the Hackney Wick part of the Queen Elizabeth Olympic Park, while not directly involved in the majority of the change envisaged remain a key partner.

INFRASTRUCTURE: PROJECTS, DELIVERY AND PHASING

Infrastructure Table

Table 2 below provides a summary of the projects that are identified as necessary or desirable to achieve the outcomes envisaged for Hackney Wick and Fish Island identified in the Local Plan and reflected in this SPD. The individual components in this list are also reflected in the Legacy Corporations Infrastructure Delivery Plan Project List which is annually reviewed and updated and is available on the Legacy Corporation website. Where specific elements of infrastructure are directly related to the application site in question, there will be an assumption that delivery, whether direct delivery or through financial contribution, will be secured by S106 Agreement. Where there is not such a direct link, or where that item of infrastructure is identified on the Infrastructure List (Regulation 123 of the CIL Regulations 2010 as amended), any financial contribution to that infrastructure delivery will be made through the allocation of CIL monies collected by the Legacy Corporation. In such circumstances S106 Agreements may still be used to secure any non-financial aspects of delivery, including timing/phasing.

Public transport improvements

There is a need to keep bus routes and service frequencies under review and taken into account as specific development proposals come forward.

A proposal for an eastern spur to Crossrail 2 which would include a potential station in Hackney Wick has been made by the London boroughs of Hackney, Newham, Barking & Dagenham and by Essex County Council. Any future safeguarding for this route and effects would be taken into account in a future Local Plan review.

CIL AND S106

Community Infrastructure Levy

The Legacy Corporation CIL charge came into force in April 2015. The current approved CIL rates can be found within the Charging Schedule at [INSERT WEBLINK]. There will be the potential for CIL monies to be allocated to infrastructure projects within Hackney Wick and Fish Island where projects have been included on the Infrastructure Delivery Plan Projects List. It should be noted, however, that CIL monies will provide only a limited level of funding in comparison to overall infrastructure costs and so many of the projects identified in this SPD will also need to consider the availability of other funding sources.

Planning permission and S106 Agreements

Where schemes are compliant with the policies and proposals set out within the Legacy Corporation Local Plan (2015), these will be most likely to contribute positively towards achievement of the overall vision and strategy for Hackney Wick and Fish Island set out in the adopted Plan.

Schemes that meet these requirements will be more likely to receive planning permission and where necessary conditions will be placed on any planning permission to ensure that those schemes continue to meet those policy requirements as they are built and then occupied.

Where relevant to a particular scheme, Section 106 Legal Agreements will be used to secure affordable housing, the provision of managed and/or low-cost workspace and physical and social infrastructure along with any other relevant matters that meet the legal tests, that is:

Where an element of infrastructure is identified on the current published Infrastructure List (CIL Regulation 123), financial contributions will not be secured through a S106 Agreements. In determining the relevant level and type of infrastructure or other provision secured through a S106 Agreement that requires a financial contribution or has a financial effect on the scheme in question, the overall viability of that scheme and the extent to which the scheme as a whole meets aspirations of the strategy set out in the Local Plan, will be taken into account.



Table 2: Identified Infrastructure

This table outlines the elements of infrastructure that have been identified for Hackney Wick and Fish Island as part of the Legacy Corporation Infrastructure Delivery Plan. It should be noted that the IDP Project List is updated annually and is available in its most recent form on the Legacy Corporation website.

Infrastructure and project description	Delivery body and funding	Timing/phasing of delivery
1. Strategic Transport Scheme Hackney Wick London Overground, station access improvements and new north south underpass underneath the railway embankment	Cost approx.: £23 m. Delivered by LLDC/ Network Rail. Funding sources – TfL, Network Rail, LLDC, inc. committed S106 and CIL Planning permission has been granted.	High priority. Start of works late 2016, completion by mid-2017
2. Local Transport Scheme: Improve pedestrian and cycle route under the A12 from Eastway to Mabley Green	No funding identified. Potential funding sources s106 / TfL / LLDC.	2021 onward
3. Local transport scheme: Further upgraded pedestrian / cycle connection over the A12 from Wallis Road to Cadogan Terrace.	No funding identified. Potential funding sources s106 / TfL / LLDC.	2021 onward
4. Local transport scheme: A new link between Fish Island North and Fish Island Mid to provide a more direct route between the hub at Hackney Wick and Fish Island Mid (includes Bridge over Hertford Union Canal)	Cost £4.6 m. Funding through s106.	High priority: 2016-2021
5. Add Ramp to south of Old Ford Lock for use by cycles and wheelchairs	£232,000. Delivery by CRT/LLDC.	2021 onward
6. New pedestrian/cycle connection from Crown Close/Wick Lane to Greenway (ramp/ stairs)	Delivery as part of development associated with Local Plan Site Allocation SA1.5	Timing of delivery to be associated with any future potential development scheme.
7. A12 underpass improvements Wick Lane	Not currently funded. Cost approximately £500,000. Delivery by LB Tower Hamlets/ LLDC	2021 onward
8. Upgraded pedestrian and cycle facilities at Wansbeck Road crossing	Not currently funded. Cost approximately £250,000. Delivery by LB Tower Hamlets/ LLDC .	2021 onward
9. A12 Road Bridge at Crown Close and Old Ford Road	Not currently funded. Delivery by TfL/LLDC including potential for an element of CIL funding. Cost estimate £20 million.	2021 onwards
10. Upgraded pedestrian link (new ramp) from Dace Road to the Greenway	Not currently funded. Cost approximately £600,000. Delivery LB Tower Hamlets/LLDC with potential for either CIL or S106 related funding.	2021 onwards
11. TfL Cycle Hire within Hackney Wick & Fish Island	Not currently specifically planned or funded. Estimated cost £500,000. Delivery by TfL Potential for S106 funding.	2021 onwards
12. Education - A 3FE primary school in PDZ5 - East Wick (Mossbourne Riverside Academy)	Has funding and planning permission. Opened September 2015 with 30 places on a temporary site, due to move to completed new building in September 2016 and admit 90 pupils each year. Funding LLDC/EFA.	School building due to open September 2016.
13. Education - A 3FE primary school, Neptune Wharf (Fish Island)	Outline planning permission. Delivery by LB Tower Hamlets or LLDC. Site secured through S106 Agreement with element of funding. Delivery not yet programmed.	Delivery after 2018.
14. Education: Provision of nurseries as part of consented new development: Legacy Communities Scheme permission: • 1 nursery at Sweetwater • 2 nurseries at East Wick. Neptune Wharf permission – 1 nursery	Delivery on site in line with planning permission phasing as part of the development schemes permitted.	Phasing related to each planning permission, with delivery requirements secured by S106 Agreement.

Infrastructure and project description	Delivery body and funding	Timing/phasing of delivery
15. Health - Primary Care Centre (PDZ 4, 2,554m2, six GPs and six dentists).	Part of Legacy Communities Scheme, with outline planning permission. Secured by S106 Agreement. Delivery by LLDC with development partner and health care provider/local commissioning group.	2018-2026
16. Health - New health facility in Hackney Wick Neighbourhood Centre	Potential for new facility in neighbourhood centre development. Need and delivery model to be confirmed with LB Hackney and local commissioning group over time. Potential to secure space through S106 Agreement.	2021 onward
17. Open space - New public open space as part of development at Local Plan Site 1,2 Allocation SA1.3: Hepscoth Road. 1.2 ha of space identified as requirement.	No planning permission or funding. Delivered as part of Local Plan compliant development scheme. Secure through S106 Agreement.	2018-2031
18. Open space - New public open space of at least 0.8 ha as part of Neptune Wharf Local Plan Site Allocation SA1.6.	Has planning permission. Delivered by scheme developer.	2016-2020
19. Open space - Canal Park along eastern edge of Lee Navigation forming part of Here East, East Wick and Sweetwater areas of Queen Elizabeth Olympic Park.	Planning permission secured and northern phase completed. Delivered by LLDC and development partners.	2014-2023
20. Play space - children's play space at Sweetwater - 1344sqm comprising 3x doorstep play and 1x local play	Has planning permission. Delivery by LLDC and development partner	2021 onward
21. Play space - children's play space at East Wick - 2501sqm comprising 4x doorstep play, 1x local play and 1x Youth play	Has planning permission. Delivery by LLDC and development partner	2021 onward
22. Community facilities - Part of Sweetwater Legacy Communities Scheme development- 2,423m2 flexible community space, 1,258m2 flexible cultural space, 3,606m2 of flexible leisure space and an Idea Store (2,460m2)	Has outline planning permission. IS requirement of planning permission and S106 Agreement. Delivery by LLDC/ Development Partner.	2021 onward
23. Community facilities - Local theatre run by 'The Yard Theatre' charity with main stage, public areas and rooms for community use.	No planning permission or funding. Delivery by site developer and Yard Theatre. Potential to secure through S106 Agreement.	Phasing dependant on wider scheme planning and delivery.
24. Sustainability - provision of heat network bridge crossings.	Delivery LLDC with development partners and relevant energy provider for Lee Navigation bridge crossings. Heat network delivery through a relevant energy provider and development scheme promoters.	2021 onward
25. Flood risk - Hackney Wick and Hackney Marshes Flood alleviation and habitat enhancement	Potential delivery agencies LB of Hackney, Environment Agency, LB of Tower Hamlets, Canals and Rivers Trust. No delivery planning currently in place. Estimated cost £7-11 million.	not yet known.

Note: where costs are known these are based on point in time information or cost estimates requiring confirmation at point of delivery. Broad indicative phasing provided which will require future confirmation of scheme delivery arrangements and timing in most cases. Identification of delivery bodies does not imply that a scheme is programmed or funded.

APPENDIX 1: GUIDANCE ON APPROACH TO ACCEPTABLE RELOCATION STRATEGIES

Introduction

Local Plan Policy B.1 requires the provision and implementation of a Relocation Strategy' in certain circumstances, in particular where there would be the loss of an existing business as a result of a loss of business premises where that site was proposed for redevelopment. One of the policy criteria within Policy B1.5 seeks the submission of a Relocation Strategy in the particular circumstances where an existing business or businesses operating at the site could not be retained as part of the development scheme and that the loss of that business from that site is deemed to be acceptable through meeting the other requirements of the policy. The following provides guidance on what is considered should be included within a robust Relocation Strategy. As each scheme will have different circumstances the actual approach to any specific relocation strategy should be discussed with the Local Planning Authority before its submission and preferably at pre-application discussion stage.

Within the process of preparing such a strategy and the structure of that strategy is recommended that the following structure and approach is used: (1) provide analysis; (2) identify a strategy; and (3) identify actions to implement that strategy.

Provide Analysis

Assessing the requirements of the businesses currently occupying the site to establish their willingness to relocate, size of business (premises and number of employees), specific business requirement in terms of location, size and type of premises required, servicing requirements (e.g. access for deliveries and pick up, utilities requirements and sizing). The analysis should also address the existing lease arrangements for the business(es) in question. Where the business wishes to cease trading rather than relocate, explain the situation and process of reaching this conclusion from the applicants point of view. This should include a statement from the business confirming that this is the position.

Identify a Strategy

Provide a strategy for relocating businesses that wish to relocate from the site, detailing how the applicant will explore relevant options with those businesses and what assistance to relocate will be provided.

Principles that relocation strategies will need to address:

1. Identifying suitable and available alternative locations that are, in order of preference (i) nearby the existing premises or at least within Hackney Wick/Fish Island; (ii) if not suitable and available relocation site available in that vicinity, then identification of suitable sites/premises within the four boroughs (Hackney, Newham, Tower Hamlets or Waltham Forest), or if no options available in those boroughs then identification within wider East London and then Greater London as whole. Where identified locations are not in the immediate vicinity of the existing premises (i.e. outside of the Hackney Wick and Fish Island area) evidence may be sought to demonstrate that the businesses to be relocated consider that the locations identified are suitable for the viable continuation of their business.
2. If there are no suitable alternative locations available at the required point in time, but suitable accommodation is available within the later phases of a nearby consented development scheme, or should the business wish to return to suitable premises within the proposed scheme itself, the strategy should examine options for temporary relocation to a site or premises available on an 'interim use' basis that would meet the needs of the business(s) to be relocated in the short term before a final move.
3. In identifying suitable sites and premises for business uses not compatible with mixed use or residential development, this should include consideration of premises, sites or locations with a suitable employment land designation i.e. Other Industrial Location (OIL), Locally Significant Industrial Site (LSIS) or Strategic Industrial Land (SIL).

4. Include consideration of opportunities for a specific relocation approach that would result in the acceptable intensification of employment use within an OIL, LSIS or SIL designated site or area nearby while ensuring that the requirements for maintaining or reproviding employment space set out within Local Plan Policy B.1 are acceptably addressed through the amount and type of business space to be provided within the application site.

5. Outline the approach to offering and agreeing relocation with those businesses that could be relocated, including principles of addressing acceptability of lease/rent costs and structure, addressing the reasonable bespoke and general costs of business relocation.

6. Specific requirements of the business(es) that would be relocated:

- Type and size of business premises
- Locational needs and preferences
- Access to existing markets for that
- Access to the premises for clients and employees and general servicing (including delivery) requirements

Identify Actions to Implement the Strategy

In principle, specific actions within a relocation strategy will be captured within planning conditions and/or S106 Legal Agreement. It will generally be expected therefore that a relocation strategy is submitted with any planning application or within a timescale that allows proper consideration of the strategy before that application is reported for decision.

Areas that are likely to require consideration are as follows and should ideally be addressed within the proposed Relocation Strategy:

- The phase of the scheme at which relocation of the business(es) would need to occur
- The trigger point within the scheme at which relocation would need to occur
- The process for and timing for agreeing the relocation site or premises with the affected business (ideally including identification of a dispute resolution mechanism).
- The type of unit, location of unit, servicing and any fit-out requirements for the business.
- Timing for availability of the site or premises to which the businesses in question would be relocated to.



APPENDIX 2: GUIDANCE ON TOWNSCAPE AND PUBLIC REALM

BUILT FORM AND CHARACTER

The built form and character of new development should: reflect the area's special qualities, distinctive character and urban grain; be simple, consistent, high quality and minimise clutter; be accessible, inclusive, safe and sustainable.

A key characteristic of the area is the dramatic steps in height between buildings and the variety of architecture and sense of craftsmanship. New development will not be homogeneous in height; it will have a variety of heights and scales allowing buildings to step down as part of the setting of heritage assets, and step up where appropriate to aid wayfinding. Successful examples of this approach are shown in the images below.

New development will deliver high quality new buildings with an appropriately simple and robust character which, within the conservation area, respond to and improve the setting of retained heritage assets.



The historic townscape of Hackney Wick and Fish Island is characterised by variety in building heights



Articulated roof expression integrated into built form (Brentford Lock West, Duggan Morris Architects)



Aggregated and stepped block massing (Housing, Belfast, O'Donnell Toumey)

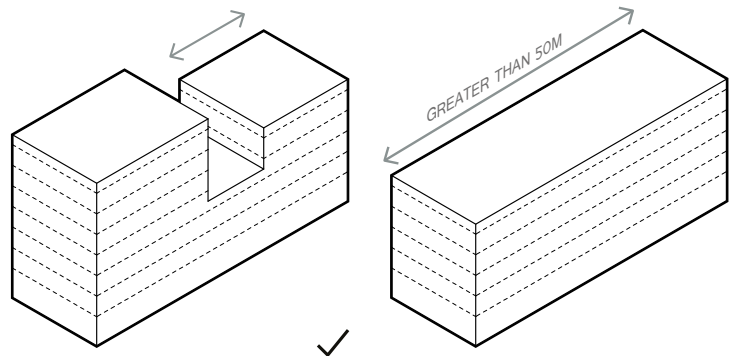


Changes in scale within a single block (Housing, Utrecht, Guerst & Schulze Architects)

The area is characterised by large urban plots composed of multiple and amalgamated buildings, and dramatic steps in height between buildings. The guidance aims to ensure new development fits in with the surrounding urban grain, providing guidance on how proposed massing should be broken up as well as how roof forms can be articulated in a way that ensures they are meaningful and express their function.

LENGTH OF FRONTAGE

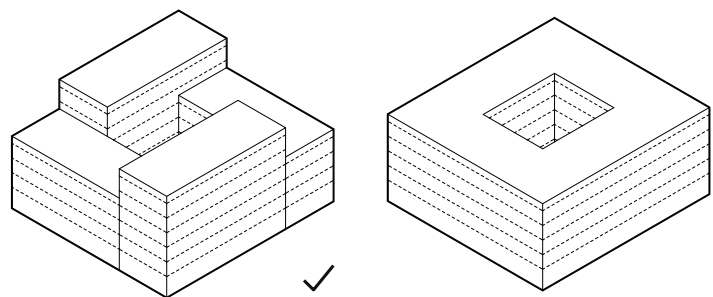
- Longer blocks must be broken down through defined breaks in massing and form. Block lengths must be limited to 50m, otherwise a break or step in massing is required.
- Development blocks should be arranged so as to create a courtyard or yard space within the block wherever possible.



Longer blocks should be broken down into more than one massing element

BLOCK MASSING AND ARTICULATION

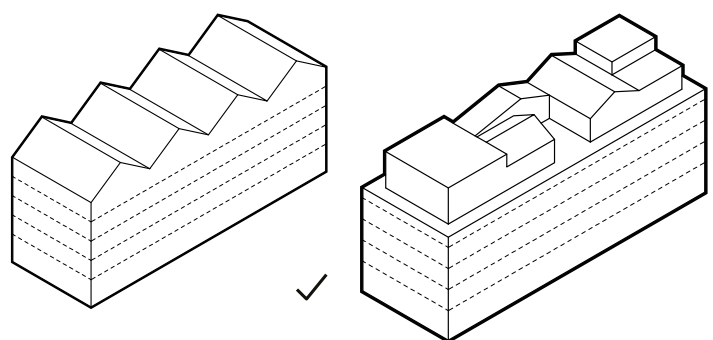
- Courtyard blocks should be articulated as an assemblage of aggregated elements.
- Changes in elevation expression must be defined and deliberate and should be accompanied by a change in building height.



Courtyard blocks should be articulated as assembled massing elements

ROOF FORM

- Articulation in roof forms should be integral to the built form.
- Proposed roof lines should be meaningful and express their function, for example roof lights providing north light for workspace.
- Rooftop additions that are articulated as independent elements, for example mansard or dormer roofs should be avoided as they are not consistent with the area's historic character.
- Elevations should have a predominantly consistent and simple roofline, which does not compete with or detract from retained heritage assets.
- Roof forms should be designed to integrate roof top PVs at an optimized orientation.
- Steps in height within and between blocks should be deliberate and purposeful, and should be a minimum of 2 storeys (6m).

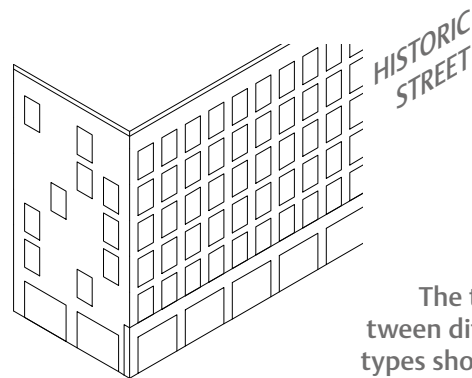


Roof forms should be articulated as an integral part of the elevation design

VARIETY IN CHARACTER

It is important that new development achieves a careful balance between creating a rich variety in character whilst ensuring a coherent and legible townscape. It is intended that variety in character and appearance will be informed by key opportunities inherent in a site. These include:

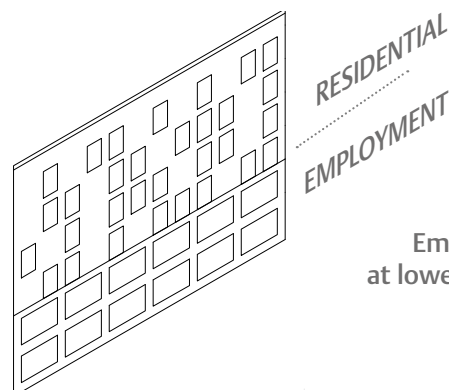
- Articulating the variety of land uses within the Proposed Development
- Responding to the distinct character of retained heritage assets
- Re-enforcing the distinct character of typical conditions and key places



The transition between different street types should be subtly articulated

TRANSITIONS BETWEEN STREET TYPES

- Careful consideration must be given to the transition between different street types within one building. Facades located on different typical conditions should have distinct elevational character whilst ensuring the whole building has a clear and legible identity.



Employment uses at lower levels should be articulated

APERTURES AND FENESTRATION

- For mixed use buildings elevations should create a subtle distinction between ground and upper level uses.
- Fenestration design should maximise daylight for proposed internal use and to create subtle variation in the façade.
- South facing elevations should employ deeper reveals to provide solar shading and mitigate overheating. Any solar shading devices must be integral to the building design.
- Variety should be achieved in the façade by subtle shifts in proportions or detailing, rather than in change of material.



Changes in window proportion should be used to bring variety to the elevations and reinforce changes in scale

EXPRESSION OF USE

- Differences between residential and non-residential uses should be reflected in the design of windows and contribute to variety in architectural character.
- Where ground floor uses have greater public access, this should be articulated in the design of ground floor frontage.



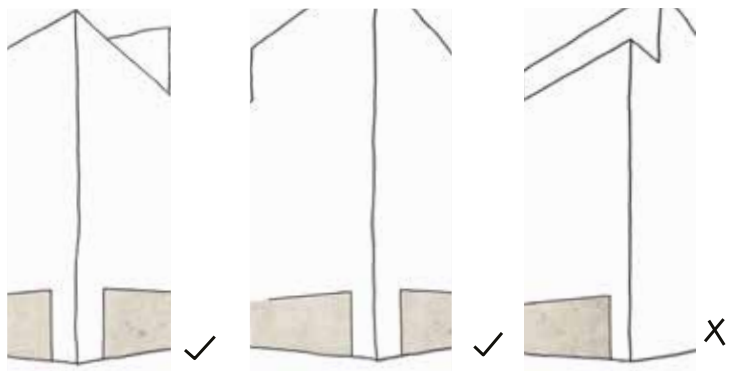
Differences in land use should contribute to variety in architectural character

BUILDING LINES AND CORNERS

New development should reflect the character of the retained heritage assets in terms of well defined building frontage at the back of the pavement line and strongly articulated corners, designed to either to celebrate an entrance or create a structural expression.

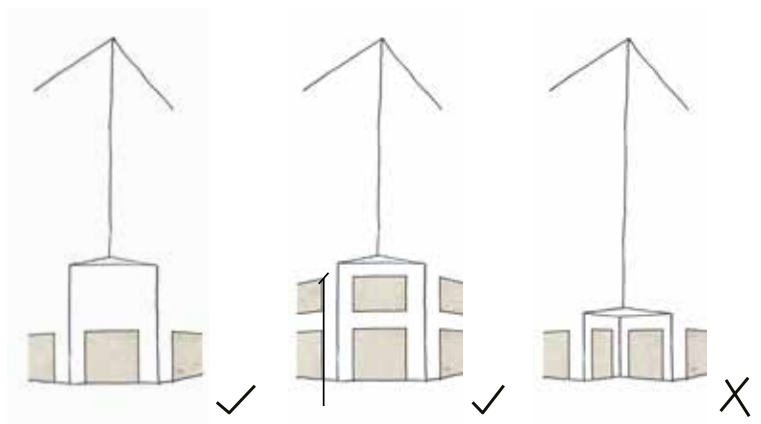
BUILDING LINES

- New development must have well-defined frontage with the building line at the back of the footway. Vehicle forecourts are not acceptable.
- Building line should follow existing curved or faceted ownership line or corners on historic streets.
- The building line of new development on the canal edge must be parallel to and set back from the river wall, but blocks should have different offset distances.



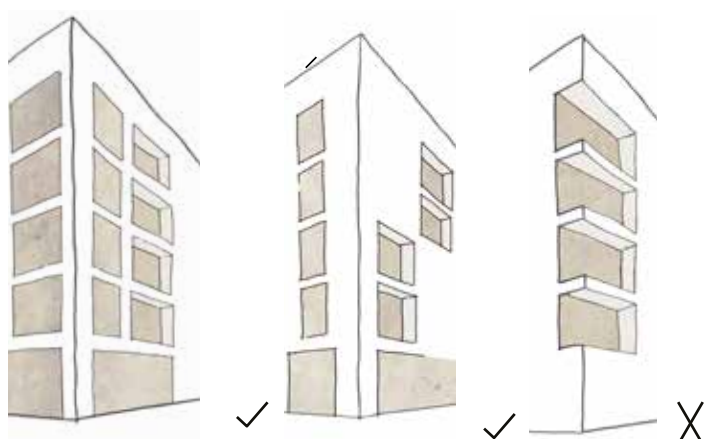
STRONG CORNERS WITH ACTIVE FRONTAGES

- Corners must be strong and simple in form to create well defined frontage onto the public realm.
- Any steps in height must be kept away from corners by 8 metres minimum.
- Set back elevations should have the same material and character as the perimeter elevation to avoid fragmentation of built form.
- Building corners on the ground floor should have active frontages on both sides.
- Extruded blocks with blank gables must be avoided.



TURNING THE CORNER

- Building corners should be chamfered at ground and first floor where necessary to maintain minimum pavement widths for wheelchair users and enable vehicle turning.
- Where elevations of different character meet, these must carefully negotiate around the corner.

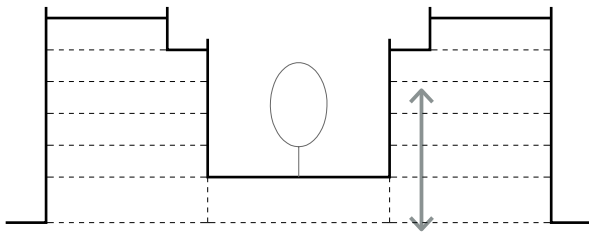


Diagrams illustrating acceptable and unacceptable approaches to corner conditions

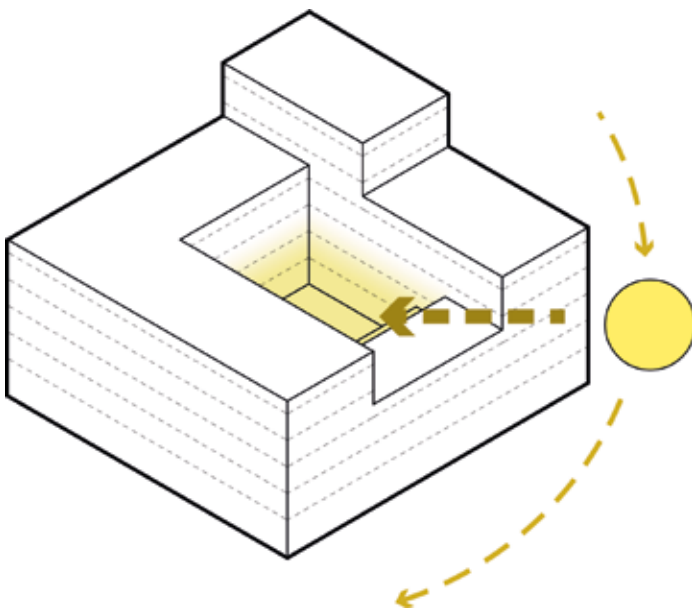
DAYLIGHT AND PRIVACY

MAXIMISING QUALITY OF DAYLIGHT IN APPROACH TO MASSING AND BUILDING DESIGN

- Set backs on the courtyard or working yard facing side of blocks should be proposed to maximise the quality of daylight to residential units and courtyards.
- Cuts in massing should be incorporated into the southern side of blocks to maximize daylight penetration into residential courts and working yards.
- The position, scale and form of cuts in massing must be designed to contribute to creating a character and urban grain for the built form that is appropriate to the Conservation Area, as well as to comply with sunlight and daylight requirements.



Set backs should be employed on the top floor of courtyard facing frontages to improve daylight penetration to residential courtyards (Please refer to 1.5.1)



Steps in massing should be employed on the south side of blocks to improve daylight penetration to residential courtyards (Please refer to 1.5.2)

Elevation design and layout will need to balance good quality of daylight with appropriate privacy. It is important that mitigation measures are integral to the character and appearance of proposed buildings and not seen as additional elements that bring unnecessary complexity to the elevation.

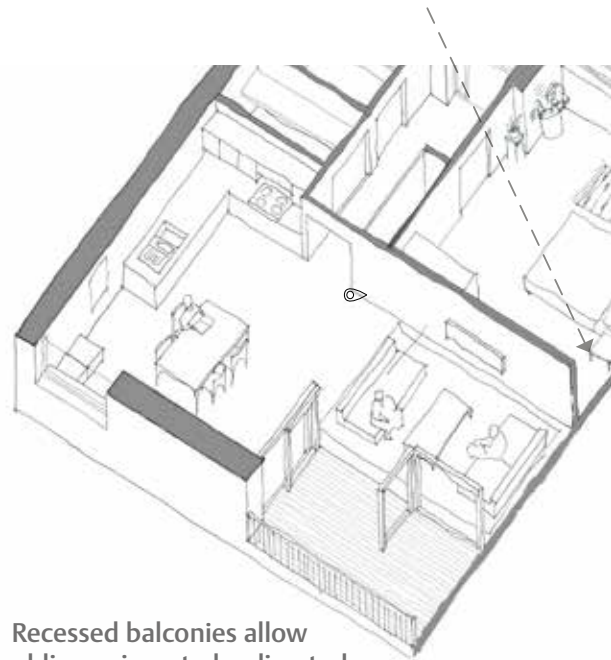
The guidance below relates to the façade design and layout of buildings fronting narrower passages of 11m and less.

FACADE DESIGN

- The use of obscured materials such as glazed bricks, and translucent glass channels should be considered to permit daylight whilst mitigating overlooking
- Oriel or bayed windows should be considered to direct views obliquely. These should only be employed if they can be successfully integrated into the facade composition and not appear as additional or unnecessary elements.
- Lighter and more reflective materials such as glazed and lighter bricks should be used in narrower passages, yards and courts to maximise the quality of daylight at lower levels and within the public realm
- The design, spacing and depth of balcony railings should be designed to maximise daylight to balconies whilst maintaining privacy. Where vertical railings are used, fins of 80-100mm in depth at 80mm centers should be used.
- Shutters and screens should be considered as a means of mitigating overlooking issues if they can be meaningfully integrated into the facade design and positively contribute to the character of elevations.
- The size and location of windows must be considered in the context of neighbouring development plots. Where RMAs have been approved for blocks opposite, proposals should seek opportunities to offset windows to mitigate overlooking issues.

BUILDING LAYOUT

- Shallower plan habitable rooms should be used to allow greater flexibility in the spacing and location of windows on the elevation
- Recessed balconies should be used to create a buffer between living spaces and the street as well as directing views obliquely along the street. Where used, recessed balconies should maximise the amount of glazing between the interiors and the balcony space.



Recessed balconies allow oblique views to be directed along streets



Use of shutters to mitigate overlooking whilst permitting daylight (Paris Housing, Herzog and de Meuron)



Lighter facade materials can bring more light into narrower passages, yards and courts



Use of translucent or obscured materials to allow light whilst addressing overlooking (Garton House, London, Colquhoun + Miller Architects)



Oriel windows direct views along streets (Bridport House, Hackney, Karakusevic Carson Architects)

PRIMARY FACADE MATERIALS

The careful selection and detailing of the primary materials of proposed buildings is necessary for the building to make a positive contribution to the public realm. The detailing of interfaces between materials is as important in creating a sense of quality as their selection. Brick and masonry should be the primary facing material for new development - if there is a strong justification other materials may also be considered.



High Quality Brickwork balanced alongside delicate metalwork (Uitzicht Housing, HDVN Architecten)



Pigmented Concrete Cladding bringing warmth and depth (Vilaverde Housing, David Chipperfield Architects)



Combining glazed and regular Brickwork to manipulate light and texture (Saxon Court Housing, Maccreanor Lavington Architects)



Decorative Brickwork bringing texture and variety (Spa School, AOC Architects)



Pigmented Zinc Cladding responding to the tones and textures of the surrounding historic context (Jewellery Studios London, DSDHA Architects)



Panelised cladding with considered detailing (Housing, Wandsworth, Sergison Bates Architects)

PRIMARY FACING MATERIALS

- A simple and restrained palette of primary materials must be employed.
- Brick and masonry must be the predominant facade material. Other materials may only be used as the primary facing material if there is strong justification.
- Timber cladding, lightweight composite cladding systems and low quality materials should be avoided.
- Recycled, reclaimed and locally made materials should be used where possible.



High Quality Brickwork with Precast Concrete Elements (Housing, Nijmegen, Biq Architecten)



Copper Cladding (Claredale Street Housing, Karakusevic Carson Architects)



Glazed Ceramic Cladding (Housing, Wil, Switzerland, Meier and Hug Architects)



Terracotta Cladding (Art Gallery Walsall, Caruso St John Architects)

DETAILING & FACADE DESIGN

- Window reveals should generally be at least a full brick deep or flush with the facade to bring depth and definition to the elevation.
- Deeper window reveals should be used, particularly on south facing elevations to provide solar shading and reduce cooling demands.
- Details such as the bonding of masonry elements, selection of masonry products, the selection of appropriate mortar colours and the integration of movement joints should respond to retained heritage assets and create a sense of robustness, solidity and permanence.
- Attention should be paid to the setting out and modulation of the facade elements to ensure an elegant and composed character.
- The detailing of primary elements should express an element of craft in the construction of the building in keeping with existing buildings in the area. For more detail please refer to next section.
- For buildings that can be viewed from a distance, the proposed scale and modulation of facade elements should take this into account for example creating larger apertures.



Considered detailing of material interfaces (Amsterdam Housing, Wingender Hovenier Architects)



Mesh Shutters manipulating weight and solidity (Paris Housing, Herzog and de Meuron)



Window Reveals bringing a sense of depth and robustness (Amnesty International Building, Witherford Watson Mann Architects)



Textured Brickwork detailing adding a sense of craft (Housing, The Hague - Geurst & schulze Architects)

SECONDARY ELEMENTS

Secondary elements such as windows, metalwork, balconies, signage and screens offer opportunities to introduce craft and refined ornament into the design, celebrating Hackney Wick and Fish Island's tradition as a place of creativity and making. Locally made materials and products should be used where possible to promote the talents of local businesses and inform a distinct identity and character. Cast materials such as pre-cast concrete and ceramics are also encouraged where their manufacturing process contributes to a distinct identity.



Galvanised Steel Staircases, Anthony Gormley's Studio, David Chipperfield Architects



Perforated and ornamental refuse storage (Housing, Brentford Lock West, Duggan Morris Architects)



Decorative Steel Railings, (Västra Kajen Housing, Tham and Videgard Architects)



Considered detailing of the interface between materials and elements (Housing, Asmterdam, HVDN Architects)



Timber ground floor frontage (Housing, Asmterdam, HVDN Architects)



Integrated solar shading within window design (Care Home, Brussels, Sergison Bates Architects)

CRAFT AND ORNAMENT

- Ornament should be integral to design of secondary elements and integrated into the design of the building.
- Recycled, reclaimed and locally made and manufactured materials and products should be used where possible.
- Carefully considered and subtle design and detailing incorporating narrative within secondary elements is encouraged.
- Crude or simplistic use of motif and pattern should be avoided.



Cast Iron Cladding introducing craft and grain (Paul Smith Store, 6A Architects)



Patterned Precast Concrete responding to a historic context, (Nottingham Contemporary, Caruso St John Architects)

FENESTRATION

- Window systems should be selected with simple and discrete profiles to avoid adding unnecessary complexity to the facade design.
- The finish of windows should be carefully considered to compliment other secondary materials such as railings and balustrades.
- Adequate natural ventilation must be integrated into the façade design.



Industrial Style Critall Windows, (Piraeus Housing, Hans Kolhoff Architects)



Integrated ventilation within window design (Europaallee Baufeld E, Caruso St John Architects)

BALUSTRADES AND RAILINGS

- Balustrades and railings must be an integral part of the faced design, glazed railings and handrails must be avoided.
- The design of railings must be designed to ensure adequate privacy for balcony spaces.
- Balcony and railing fixings must be discreet and hidden wherever possible to avoid clutter in the elevation design.
- The finish and color of architectural metalwork should be selected to compliment the primary facing material and be considered alongside finishes of windows and other elements of the facade.



Delicate balustrade detailing (Charlotte Street, Stephen Taylor Architects)



Decorative Steel Balustrades, (Townhouse in Munich, Hild und K Architekten)

ENTRANCES AND SIGNAGE

- Particular care must be taken in the design of elements where users come into contact with the building, to ensure durability and comfort. In these locations should include softer, more tactile and robust materials.
- The design of signage should balance the need to be integrated into the building design whilst expressing the variety. Signage must be considered at an early stage in the design.



Ornamental Stonework (Museum of Childhood, Caruso St John Architects)



Tactile entrance materials (Housing, Frankfurt- Stefan Foster Architects)

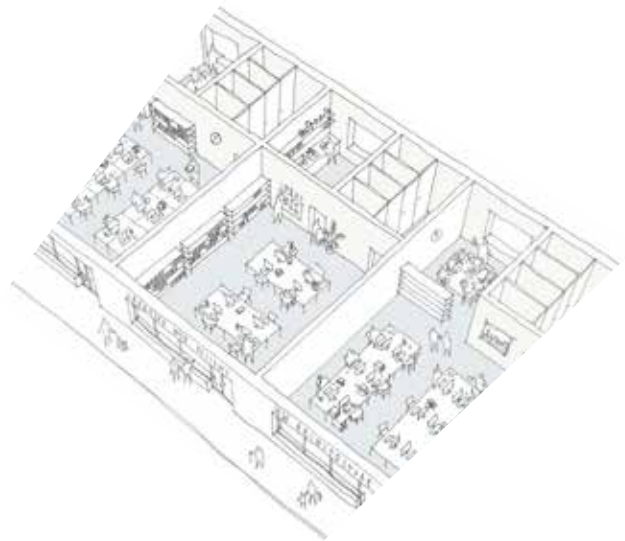
WORKSPACE DESIGN

The design of new workspace should be carefully considered in relation to the type of occupier envisioned. A balance should be achieved between meeting the needs of current anticipated occupiers whilst allowing sufficient adaptability to allow change over time.

A broad range of new accessible and flexible workspaces will be provided to address the shortage of accessible employment space in the surrounding areas.

This guidance establishes three key occupier types and the associated design requirements for each; the occupier addressing issues such as floor to ceiling heights, layout, natural daylight and servicing.

If flood risk is relevant to a site, all non- residential ground floors must be designed to be flood resilient, and basements must be flood proofed.



Office and Workspace



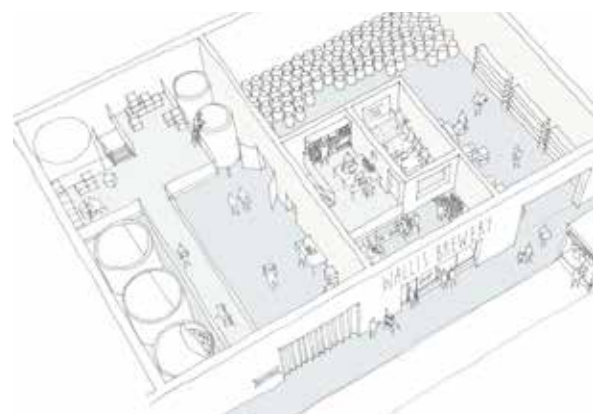
Creative Low Cost Workspace



Creative Making Space, Sugar House Studios, London



Office space, Dalston



Small-Medium Scale Light Industrial

B1a WORKSPACE

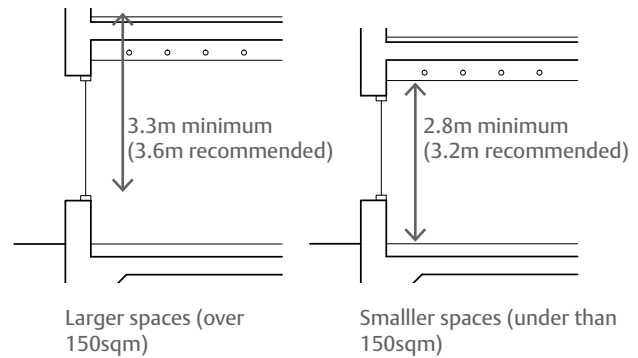
Commercial workspace should be designed to allow easy future sub division into smaller units.

Floor to ceiling heights must be a minimum of 2.8m for spaces less than 15m² and 3.3m for larger spaces.

Spaces should be well proportioned and orthogonal to allow for flexibility of layout and occupation.

Adequate service zones should be provided either under floor or under ceiling to allow for distribution of services that can accommodate a range of commercial occupiers.

Internal uses that create animation (for example reception area or meeting rooms) should be located on the frontage to animate the public realm. Staircases and light wells should be avoided or only take up the minority of proposed frontage.



B1c CREATIVE LOW COST WORKSPACE

Floor to ceiling heights must be a minimum of 3.2m at ground floor and 2.75m at upper levels. Higher floor to ceiling heights should be provided for uses that involve the use of larger objects and equipment.

Adequate wall space must be provided for the display and hanging of work.

The design of communal circulation and yard spaces should facilitate informal use and exchange between occupiers.

Communal corridors should be at least 1800mm wide to allow delivery and transportation of a range of objects and equipment.

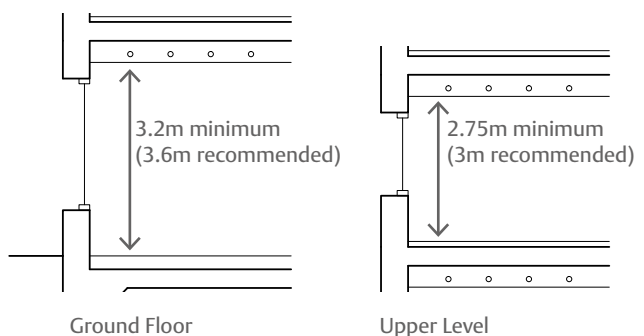
Spaces must be configured to maximise north facing aspect and opportunities sought to integrate north facing roof lights when located on the upper floors.

Adequate servicing must be provided to allow intermittent access and deliveries from small vehicles (including vans).

Flue extracts must be integrated into the design of the building.

Partition and party walls must be of a robust construction such as blockwork.

For multi-storey buildings, suitably sized goods lifts should be incorporated.



B1c SMALL-MEDIUM SCALE LIGHT INDUSTRIAL

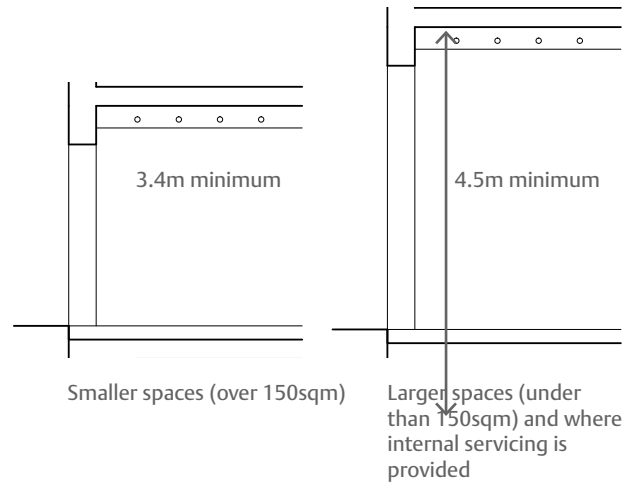
Attention must be given to the arrangement of access and servicing to ensure uses can be serviced without detriment to the public realm or adjoining residential units.

Flue risers must be integrated with residential core layouts above to ensure all servicing can be integrated into the internal layout of buildings.

An adequate ceiling zone must be provided at the interface of B1c and residential uses to provide reasonable acoustic separation as well as space for service distribution and transfer structures.

Floor to ceiling heights must be a minimum of 3.4m. If internal vehicle access is required then ceiling heights should be increased appropriately.

Where regular vehicle access is required this should be located from dedicated yard spaces or via an internal loading bay.



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WORKSPACE FRONTAGE

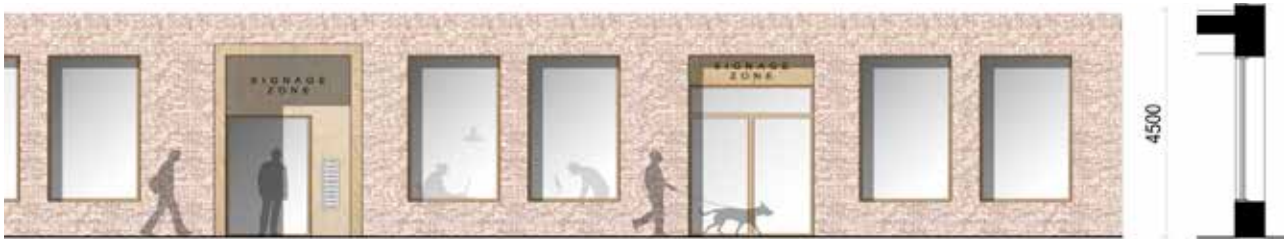
The variety of workspace occupiers and typologies will contribute to the character of the area. Ground floor workspace will need to balance active frontages and daylight with adequate privacy and practical internal arrangements. Wherever practically possible, workspace is encouraged to have a visible presence on the street and provide a 'window into working' in the area.

It is possible that some ground floor workspace will change over time to include more public facing ancillary A class uses. In these areas, the design of frontages should allow for future adaptation with flexibility to create a more open frontage. Note that this would be subject to a separate planning application.

Workspace signage is seen as an opportunity to give visibility and a clear and legible identity to the variety of businesses in the area, with opportunities for individual expression, craft and ornament.



Ground floor office creating a positive frontage to the street (Office building, Zurich - Max Dudler Architects)



Illustrative Workspace Elevation achieving a balance of active frontages with appropriate privacy to ground floor uses



Illustrative Elevation of workspace capable of future adaptation to A class frontage. Facade areas between brick piers should be designed to allow easy replacement with shopfront glazing in the future.

← Area with potential for future adaption →



Illustrative Yard Entrance Elevation integrating legible wayfinding devices and signage whilst allowing views into yard spaces behind

WORKSPACE FRONTAGE

Ground floor workspace frontage must be designed to achieve reasonable privacy whilst allowing occupiers to activate and overlook the public realm to maintain a good quality workspace environment.

Locations for ground floor windows must be carefully considered to animate the public realm.

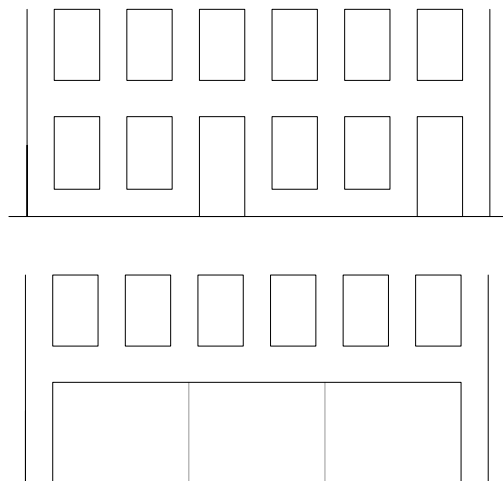
Frontages to the public realm must incorporate raised window cills to create privacy and to allow for flexible planning of workspace interiors.

Frontages to working yards must be more open and to allow opportunities for activities and uses to spill onto yard areas, and incorporate larger openings such as double height doors and roll shutters.

Entrances to individual workspace units and workspace clusters must be visible from the street.

Where Parameter Plans allow flexibility between A and B uses the design of workspace frontage must reflect the proposed use.

Workspace units may be double fronted onto yard or courtyard spaces, particularly where these spaces are proposed to be visible and/ or accessible to the public, provided there is no conflict with the quality of environment for upper floor uses, in particular for residents.



Ground floor frontage must be framed between elements of the building facade



Using depth to create an appropriate balance of privacy and openness within ground floor workspace (Amnesty International Building - Witherford Watson Mann Architects)

WORKSPACE SIGNAGE

For workspace located around yards, yard signage should be provided on street identifying the occupiers.

At Reserved Matters Application stage, a coordinated signage strategy must be developed, establishing common datums and fixing heights whilst allowing appropriate scope for variety between individual businesses.

Workspace signage should contribute to area's identity as a place of creativity and making, by sense of craftsmanship and ornament in their design.

Signage should be integrated into the design of architectural elements such as metalwork and joinery.



Yard entrance design (Illife Yard, London)

A CLASS FRONTAGE

Shopfronts play a key part in our perception of shopping streets because they are at eye level and are accessible to everyone. When considering installing a new shopfront it should not be seen in isolation, but in the wider context of the building and the street in which it is located. The character and proportion of upper floors and surrounding buildings must be taken into account in the design of new shopfronts.

The public nature of A class frontages mean they will contribute to the character of public spaces. There is a need for a coordinated approach to frontage and signage design to ensure ground floor public uses are legible and coherent. Businesses and uses are encouraged to express their public function and bring definition to key public spaces and routes, in a coordinated manner which avoids clutter.



Use of tactile and robust materials, Dublin
(Lawrence and Long Architects)



Signage as an integral element of facade design, Nijmegen



Retail activities spilling onto the public realm, Boundary Estate, Hackney



Projecting signage providing presence along oblique views, Blackhorse Lane



Integrated retail canopy, Shoreditch Hotel



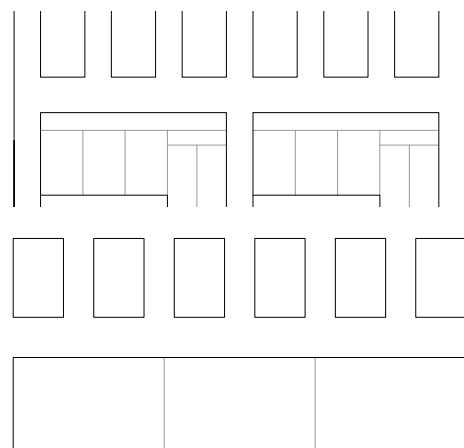
Illustrative A class frontage showing principles for signage setting out

FRONTAGE

- The shopfront should sit within the building framework set by structural columns and beams and should carry significant features through to ground floor level, rather than visually separating the ground and first floors.
- The vertical and horizontal emphasis of the surrounding buildings is important when designing a new shopfront. If a unit occupies more than one building, the vertical division between the buildings must be retained. In addition the scale and proportion of the shopfront and the elements within it need to correspond to the scale of the building above. Designs need to be imaginative within the constraints of the existing building, enhancing its character rather than conflicting with it.
- Careful attention to detailed design should give the shop a unique quality which will help promote its image and consequently the goods inside.
- Canopies and blinds protect goods from damage by sunlight and provide shelter for shoppers in bad weather. Blinds should always be retractable, and their housings should not obscure any attractive features of the shopfront. Blinds should be made of canvas or a similar non-reflective material, not plastic.
- The need for security shutters should be considered at an early stage when planning a new shopfront. Solid external shutters can have a deadening effect on the character of the shopping street. When they are down they obscure details of the shopfront, the window display is not visible and additional illumination for the street is lost. Where security is a problem, grilles of an 'open' design may be acceptable in some locations but laminated glass or internal shutters are the preferred solutions.
- Ground floor openings must be framed within elements of the building facade to ensure the ground floor is integrated into the rest of the elevation.
- At public spaces and areas of increases footway depth, opportunities must be sought to allow retail and cafe uses to spill onto the public realm without presenting as a hazard, especially for people who are blind or partially sighted.
- Shop glazing must generally be fixed to allow for window displays behind.
- Security shutters, projections and awnings must be integrated into the facade design to avoid cluttering of the streetscape.
- Background ventilation intake and extract must be carefully integrated into retail frontage to avoid negatively impacting on the appearance of shop fronts. Integration of seating into the facade design is encouraged to activate the public realm.

SIGNAGE

- Workspace signage should contribute to the area's identity as a place of creativity and making, by sense of craftsmanship and ornament in their design.
- Opportunities must be sought to bring a sense of craft and ornament in the design of signage to allow uses to contribute to the areas identity as a place of creativity and making.
- All signage must be fixed on a clearly defined signage area located between the ground and first floor.
- Signage must be coordinated in way that avoids unnecessary clutter in the design of ground floor frontages and promotes legibility.
- Any projecting awnings must be located above the fixed signage zone and contain integrated signage.
- Branded adhesive films must be avoided in favor of fixed signage located in a defined signage zone.
- Projecting signage must only be used where it significantly improves the visibility of businesses when viewed obliquely, is part of a coordinated signage strategy and does not add unnecessary complexity to the elevation.
- A coordinated signage strategy must be developed, establishing common datums and fixing heights whilst allowing appropriate scope for variety between individual businesses, particularly those on historic streets.



Ground floor frontage must be framed between elements of the building facade

A CLASS DESIGN

Retail A class uses are located fronting key spaces and along key routes where there is maximum footfall and visibility, particularly on historic streets and the north south route. These uses have the potential to create a dynamic relationship between the public realm and the occupation of the ground floor.

A strong presence on the street should be created by maximising opportunities for uses to spill out onto key public spaces including café and restaurant seating and display of produce from retail units.

Retail units should be designed to be visible, flexible, functional and lettable so they can positively contribute to the neighbourhood centre.



A class uses offer an opportunity to showcase and promote and showcase activities in the area



Uses spilling onto the public realm, Exmouth Market, London

GENERAL GUIDANCE

- Retail and Cafe uses should be clearly visible from the public realm.
- Refuse storage should be carefully considered and discreetly integrated to minimise its visual impact onto the public realm.
- The minimum floor to ceiling heights should be 3.2m however 3.5m is recommended to ensure spaces are lettable and capable of easy adaption by occupants.
- Canal fronting cafes and bars should provide increased floor to ceiling heights to maintain a strong relationship to the canal edge and allow for flexibility of use.
- Canal fronting cafes and bars should allow space for outdoor seating.
- Back of house, storage and ancillary spaces should be located in the block to maximise the extent of active frontage onto the street.
- Retail units should be designed with sufficient flexibility to allow for the combination or subdivision of spaces over time. To optimize this, slab levels must be kept consistent wherever possible.
- Spaces must be orthogonal and well-proportioned to accommodate a range of occupiers and allow for flexible internal layout.
- Where restaurant and café uses are proposed, kitchen ventilation and flue extract should be integrated into core risers and discharged at roof level to ensure they have no visual impact onto the street.
- A class units may be double fronted onto yard or courtyard spaces, particularly where these spaces are proposed to be visible and/or accessible to the public, provided there is no conflict with the quality of environment for upper floor uses, in particular for residents.

BASEMENTS

Basements often provide useful ancillary space, and if designed well- can provide high quality work and retail space, as part of a balanced provision of uses, and are only acceptable if the following can be demonstrated:

- Evidence of sufficient need or demand for the proposed basement employment space.
- The basement design does not prevent creation of active frontages, particularly on historic streets and the north-south route (for example frontage dominated by light wells or circulation).
- The provision is part of a mix of types of accommodation as part of multi-storey employment space.
- The provision is part of a mix of types of space across the neighborhood centre.
- The provision is for ancillary uses such as storage, meeting spaces, toilets etc. only or to meet demand for photography or music studios or specialist activities that do not require daylight.
- The provision is accessible and serviceable, for example via a goods lift.
- The provision is flood proof.



Basement workspace will be suitable for a limited number of uses which do not require natural daylight such as photography studios and recording spaces.



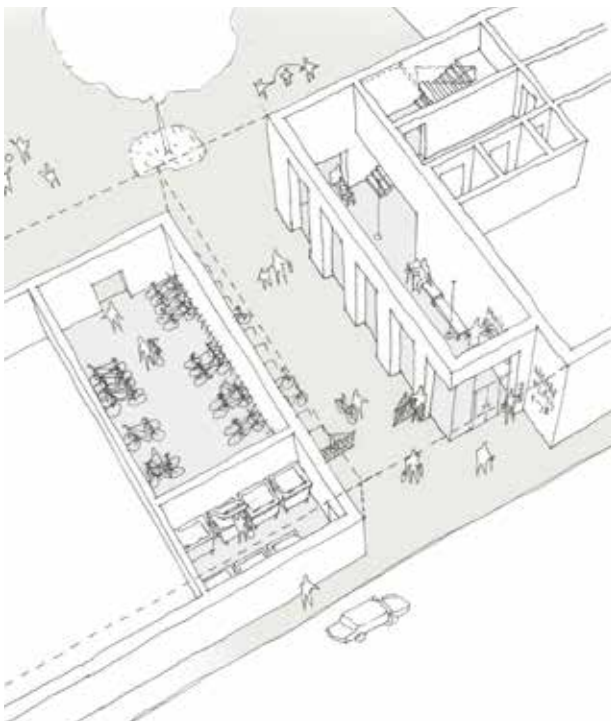
Public uses can be located in semi basement areas if they maintain a good relationship with the street and are provided with a reasonable level of daylight

In mixed use areas, proposed development requires special consideration in the residential access and entry sequence and offers opportunities to articulate a transition from public spaces to environments with a more domestic quality. Access to residential units will be provided from legible entrance lobbies accessed from the street or from residential courts.

Residential access areas will need to be well overlooked with a requirement for entrance lobbies to be located adjacent to court entrances.



Robust but tactile materials are encouraged where residents come into contact with the buildings (The Piraeus Building, Amsterdam, Hans Kolhoff Architects)



Illustrative example of access into a grade level residential court with adjacent entrance lobby



Residential courts will have a distinct domestic quality

ACCESS THROUGH AT-GRADE RESIDENTIAL COURTS

Blocks with ground floor residential courts must use the courtyard as an integral part of the entry sequence to residential units to ensure a strong connection to the courtyard for all residents

Gates must always be placed on outer edge of the building to avoid recesses in the public realm

Gates must be designed to allow visual permeability so a visual connection is created between the street and the courtyard.

Where the building mass overshalls court and yard access, linings and soffits to courts must be finished with high quality materials to create a strong sense of arrival

Cycle stands should be integrated within covered areas to create convenient and inclusive cycle storage that can also accommodate non standard cycles.

Bin stores should be located off the threshold in gated courts and yards to reduce their impact onto street facing elevations

Entrance lobbies should be located adjacent to court access to provide passive surveillance and activate covered areas

Residential court access should be a minimum of 6m wide to ensure a generous entry sequence

RESIDENTIAL LOBBIES

Entrance lobbies must be located on street, with the exception of canal side locations where it may be acceptable to provide access to residential units through at grade residential courts.

Entrance lobbies must be generous and legible and create a strong sense of arrival

The design of entrances must be tenure blind with no change in appearance or specification between tenures internally and externally

The scale of entrance lobbies must relate to the number of units served. Where greater numbers of units are served lobbies must be distinctive and grand, where fewer units are served entrances must be more discreet and modest.

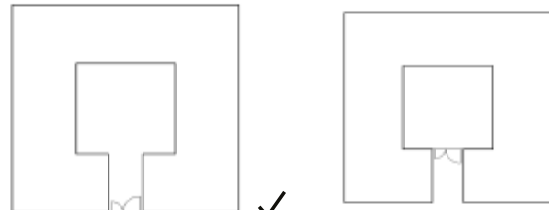
The design of internal floor and wall finishes should be robust, hard wearing and of a high quality across all tenures

EXTERNAL GALLERY ACCESS

Gallery access must only be proposed fronting onto residential courtyards and working yards and is not appropriate onto historic streets or new passages.

Where external access galleries are employed, care must be taken to create appropriate buffers between public and private, particularly where habitable rooms face gallery areas. The design of fenestration onto access galleries must achieve an appropriate balance between maximizing daylight and aspect to residential units whilst ensure adequate privacy

Sufficient defensible space at entrances must be provided allowing opportunities for appropriation by residents to create an appropriate interface between public and private.



Entrance gates must be located on the outer edge of the building



Cycle storage integrated into court access (Adelaide Wharf, Hackney - AHMM Architects)



High quality and robust materials (Residential Entrance Lobby - Guerst & Schulze Architects)



Appropriation of gallery access areas by residents, (Apartment Building, Brussels - Stekke + Fraas Architects)



Communal areas used as sociable spaces (Homes and Work for Change, Manchester - Mills Beaumont Leavey Channon Architects)

RESIDENTIAL FRONTAGE

Residential entrances offer opportunities to articulate the residential character of the neighbourhood centre at ground floor. Emphasis is given to the use of tactile materials where residents come into contact with the buildings at entrances and thresholds. The Design Code encourages a holistic approach to the design of signage, post boxes, lighting and door entry systems.

Focus is given to how ancillary frontages to bin/bike stores and plant rooms can be integrated into the facade designed with a discreet appearance.



Changes of material and integrated signage to identify entrances (Saxon Court, Kings Cross - Maccreanor Lavington Architects)



Example Residential Entrance with Street Access to the Court



Example Residential Entrance with no Street Access to the Court

RESIDENTIAL ENTRANCES

- Lobby frontages must be differentiated from non residential uses in their design to improve their legibility and identification
- Use of softer and more tactile materials such as solid timber must be considered at entrance lobbies to mark a transition to a more domestic environment
- All communal entrances must be recessed into the building mass to create adequate weather protection and projecting canopies should be avoided. Generally residential entrances should be recessed at least 600mm from the street and recesses should be angled to create better sight lines and discourage anti social behavior.
- Postboxes, intercoms, lighting, signage and all other elements of communal entrances must be integrated into the overall entrance design to maintain clarity and coherence in the ground floor facade.



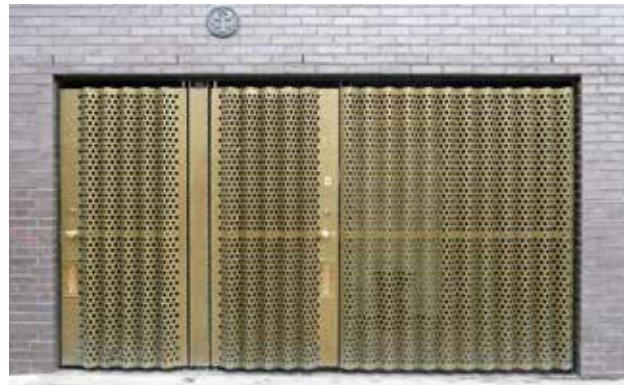
Recessed entrances creating threshold (Housing, Whitechapel - Nial McLaughlin Architects)



Use of soft but robust materials at communal entrances (Housing, Frankfurt- Stefan Foster Architects)

ANCILLARY RESIDENTIAL USES

- Bin, bike and plant stores must be designed as special elements and differentiated from residential lobbies to bring clarity to the public realm
- Decorative or perforated materials must be considered to screen louvered walls to plants, bin stores and car parks whilst allowing adequate ventilation
- Ground floors must be configured to avoid long lengths of Ancillary frontage
- Bin/bike stores and plant rooms must be separated by active frontages such as entrance lobbies or non-residential frontages and wherever possible they should not be positioned opposite one another across any public realm
- Entrances to car parks should be carefully designed and located to avoid creating a negative impact onto the street and avoid conflicts with pedestrians



Decorative metalwork to screen ancillary areas (Mixed Use Building, Hackney - Stephen Taylor Architects)

PUBLIC REALM

Whilst the area is highly valued by the local community and a hive of creativity and production, in areas it currently suffers from poor quality public realm and accessibility, resulting in a perception that it is unwelcoming to families and elderly residents living in the area. The area can be difficult to navigate, with poor legibility and dead ends. It is dominated by informal vehicle parking and servicing, and lacks a central focus and good public space.

New mixed use development provides an opportunity to enhance existing streets and canal edges, but also create new routes and public spaces. It is important that the public realm provides a high quality, consistent, calm and uncluttered foreground that supports the activities of the local community, responds to the area's distinctive character, and improves the pedestrian experience.



Hard working Public realm
(Street Interrupted, Hackney Wick - muf Architecture/ Art)



Use of robust materials
(Clink Street, Southwark - Witherford Watson Mann Architects)



Integration of hard and soft landscaping
(Bonn Square, Oxford - Graeme Massie Architects)

PUBLIC REALM OBJECTIVES



1: Improve connectivity

Proposals should seek to increase the permeability of the Neighbourhood Centre area creating new passages and public spaces, particularly public access to the station and canal edge.



2: Be accessible, inclusive and safe

Proposals should promote accessible and inclusive design and comply with the LLDC Inclusive Design Standards, creating a lifetime neighbourhood where all members of the community can live together throughout their lives with ease and in comfort.



3: Be simple, consistent and high quality; minimise clutter

Proposals should create a timeless and simple foreground that will age gracefully over time and create a sense of quality, reinforce the area's hierarchy of street including making clear of the priority given to pedestrian movement along primary routes and seeking to reduce physical obstructions and visual clutter wherever possible.



4: Reflect the area's special qualities and distinctive characteristics

Proposals should respond to the area's distinctive historic character and in terms of the hierarchy of streets, yards and passages, the character of heritage assets, reveal historic surfaces and, where possible, use materials that are locally made.



5: Be multi-purpose and provide amenity

Proposals should be designed for dual use, and opportunities for seating and occupation of the public realm should be encouraged. Streets and public spaces should be child friendly and 'playable' landscape which will encourage children and their carers to use open spaces.



6: Be sustainable

Proposals should promote sustainable design including using materials that have a low carbon footprint, maximising on site sustainable urban drainage solutions and increasing biodiversity.

GENERAL

- Footways must provide a minimum of 1.8m clear zone from back of pavement (or street furniture back of pavement) to front edge of kerb, and 1.8m clear zone behind inset parking and loading bays
- Footways must have a consistent approach to finish and colour, that are appropriate to their context as per the material and product codes.
- Footways must give priority to pedestrians, including at vehicle crossovers.
- Footways must have a consistent approach to paving bond, which should be perpendicular the direction of travel and should reinforce the hierarchy of streets, for example in treatment of street corners.
- Footways must include inset parking bays raised to footway level
- Inset parking bays should drain towards the carriageway with a 10mm upstand both at junction with footway and with carriageway.
- Footways should include flush inset service covers.
- Footways should minimise number of cuts to paving materials and have a consistent approach to paving bond, including treatment of street corners.
- Footways should break up the visual impact of parking bays by limiting on-street parking to small groups of, say, five spaces. These groups can be separated by kerb build-outs, street furniture or planting.

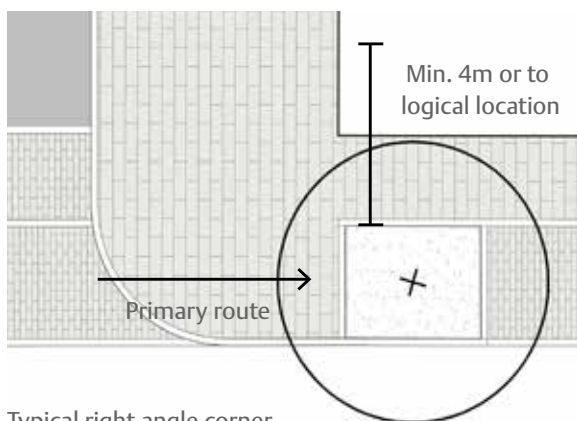
MATERIALS AND PRODUCTS

Footways:

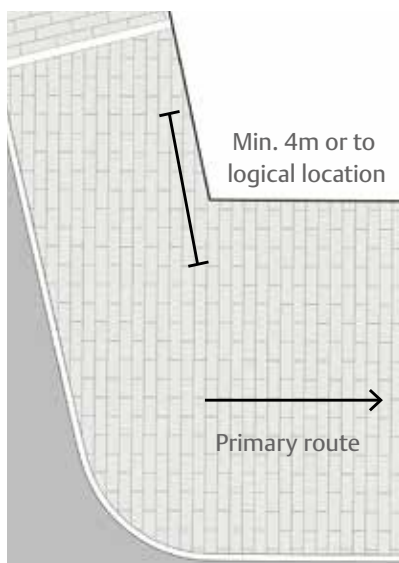
- Historic streets and passages must be laid with standard 150mm offset staggered bond.
- The stagger should be repeated every four intervals to avoid repeated patterns and give a less mechanical surface variation.
- In all situations, the bond of the footway must be at 90 degrees to the alignment of the kerb face and direction of travel.

Vehicle crossovers:

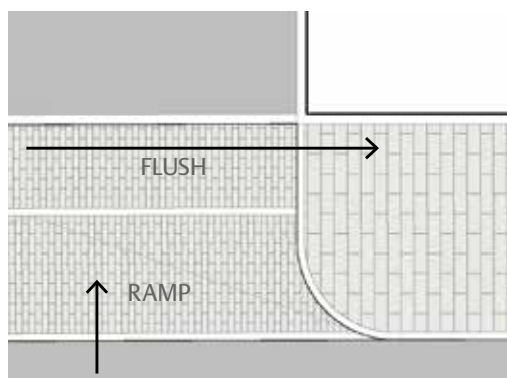
- The construction of vehicle crossovers must be laid in a stringer bond in a direction to match the larger format footway material it integrates with.



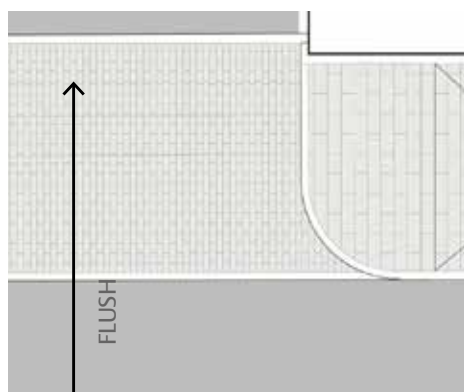
Typical right angle corner



Typical non-right angle corner



Crossover



Yard entrance

FOOTWAY PAVING ON STREET CORNERS

The bond of the paving on the historic streets should extend around the corner into adjoining passage .

The footway paving bond should not change direction on the corner, but extend 4m around the corner or to a logical position before rotating to create a neat junction with minimal cuts. A logical position could be either a vertical element within the elevation of the adjoining building, or a feature or constraint within the surface construction such as a parking bay or tree pit.

Transitions between bond directions should be cut straight not interlocked.

Transitions should not be made over service covers or other stats.

Where the angle of the adjoining kerb cuts across the paving bond there are likely to be multiple cuts to paving. In this instance, a minimum size for a cut slab should be agreed and setting out agreed paving runs to mitigate multiple small cuts and associated maintenance issues.

VEHICLE CROSSOVERS

In order to create pedestrian priority at vehicle crossovers must:

Provide level access by allowing for a 900mm minimum wide zone at footway level on all vehicle crossovers.

Use tactile paving to indicate vehicle crossover

Note: As an exception, for entrances to historic yards, which are wider than 13m, rather than the carriageway being raised to footway level to create a level crossing, the footway should be dropped to carriageway level to signify the historic significance of the yard. This is only applicable to Rubberworks Yard and Queens Yard.

INTERFACE WITH NEW AND EXISTING BUILDINGS

GENERAL

- The interface between existing and new buildings and the new public realm surfaces must be formed by a differentiated but consistent edging detail.
- Where the building footprint is not a single straight line (e.g. due to brick construction and brick piers), new footway paving should stop short of the building line and a clear zone of another paving material should be hand laid at the interface with the building with a metal edging.

MATERIALS AND PRODUCTS

- The paving material used along the building line should consist of brick, a smaller unit size or a poured material such as in situ concrete, and should relate to the materials of existing building. For example, either through using exposed aggregate or use engineering brick to match colour of the existing building.
- Interfaces between new buildings and new public realm should be constructed with a 150mm wide granite kerb



The public realm design should integrate and respond to the edges and bases of existing buildings

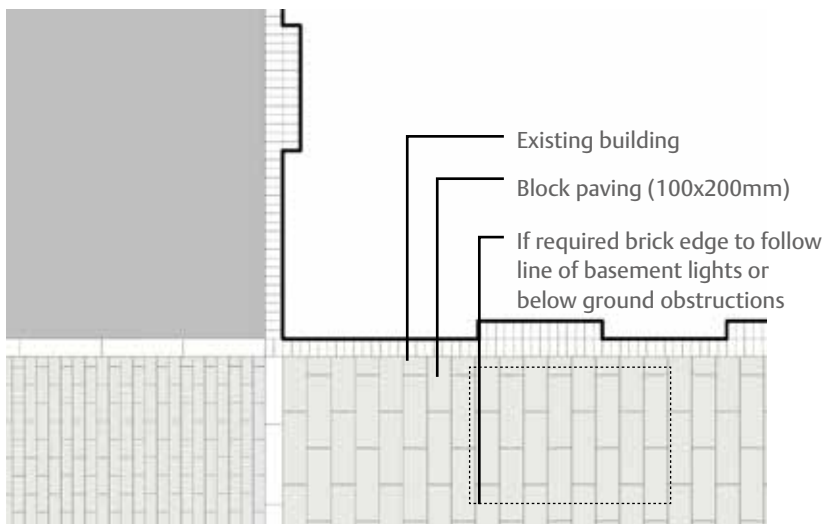


Diagram indicating paving bond only

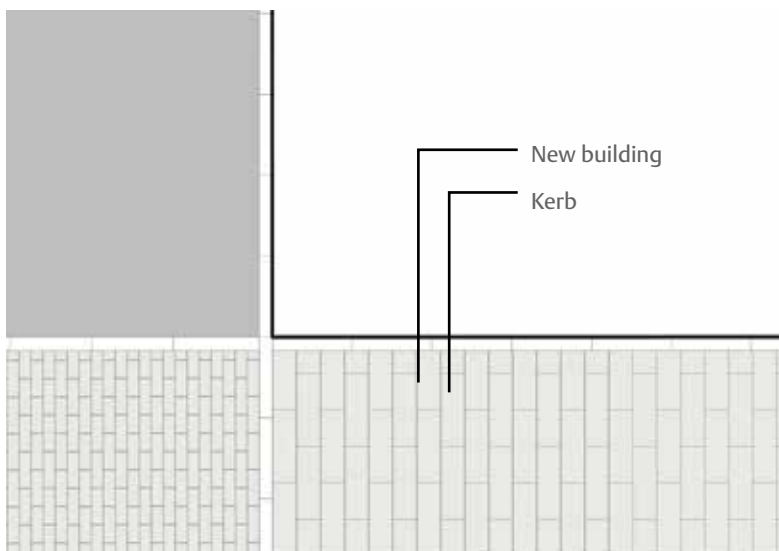


Diagram indicating paving bond only

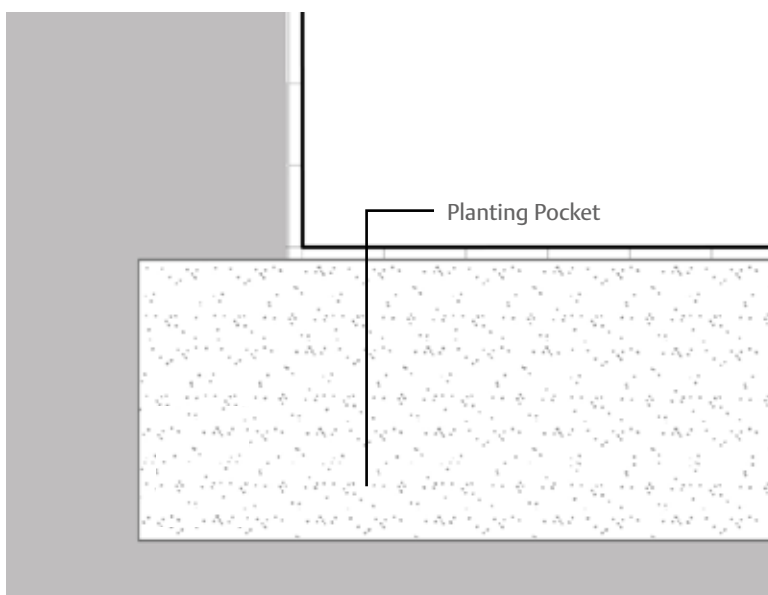


Diagram indicating typical planting pocket

Interfaces with Existing Buildings

- Where a continuous edge cannot be formed (e.g. at the interface with existing cellar lights or access to basements) the edge should step to accommodate.
- Brass studs should delineate spill out spaces where appropriate and define ownership boundaries.

Interfaces with New Buildings

- Interfaces between new buildings and new public realm should be constructed with a defined new edge constructed in a single run of 150mm wide granite kerb.
- This should form a straight edge butted to the new construction of the elevation with the new surface of the public realm material butted against the opposite edge.
- Where steps or edges in the building form require the new interface edge to not be straight then this should be constructed with butt jointed overlaps and not mitred corners.

Landscape Planting Pockets

- Planting pockets' installed along the building line (e.g. on the canal edge) should also be used to help provide greater definition to public / private realm interfaces or demarcate land ownership boundaries.
- All planting pockets should be flush and must not create physical barriers or visual clutter.

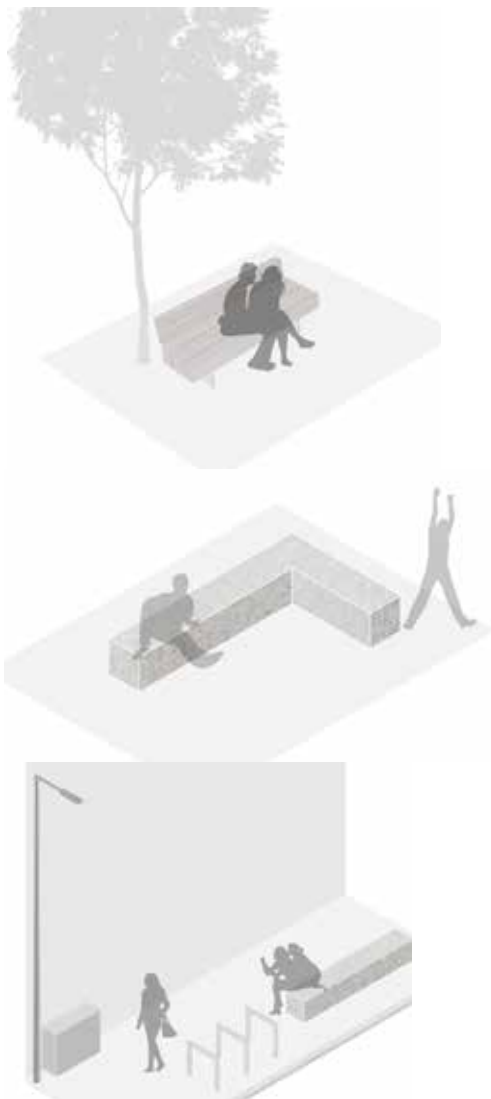
STREET FURNITURE

GENERAL:

- A street furniture audit must be undertaken before commencing detailed design to determine what existing street furniture and lighting is in place, and what should be removed.
- Street furniture must be of robust construction, resistant to vandalism and be placed in positions that minimise risk of damage by vehicles.
- Street furniture must have a consistent approach to finish and colour, that are appropriate to their context as per the material and product codes.
- Street furniture must have a consistent approach to finish and colour, and be appropriate to their context as per the material and product codes.
- Care should be taken that street furniture at the rear edge of the footway does not make adjoining properties less secure by providing climbable access to windows.
- Street furniture should be clustered into appropriate zones to avoid physical obstructions and respect pedestrian desire lines.
- Street furniture should be located where the footway widens and opens into key public spaces such as 'North-South Route' and the Canal edge.
- All street furniture should be placed to allow access for street cleaning.
- Use of bollards should be avoided.
- Planters must be built-in and flush with footway level, unless located the footway widens and opens into key public spaces such as 'North-South Route' and the Canal edge.
- Street furniture should maximise opportunities for dual functions such as lampposts being used for signage.
- Walls and retaining structures should be used for seating, rather than adding new freestanding furniture. See next chapter for more detail.
- Street furniture should be located with enough space around to fulfil its function.
- Street furniture should be located in response to appropriate sunlight and microclimate conditions, e.g. seating on the northern footway or 'sunny side of the street'.
- In ground power units and drinking fountains should be included in key public spaces (e.g. North-South Route and Boathouse Square).
- Consideration should also be given to integrating electric charge points with street furniture.



Image showing examples of simple street furniture



Diagrams illustrating formal and informal approaches to seating in the public realm



Pre cast concrete wall and bench



Brick wall with concrete coping

SEATING

- Seating must be positioned at least at 50m centres in accordance with The 'LLDC Inclusive Design Standards'.
- Seating must include space for pushchairs, prams, wheelchair and scooter users.
- Seating must be of appropriate height and contrast to immediate context to be obvious to people with visual impairment.
- In pedestrian priority areas, built in street furniture should be located in order to provide protection and definition to pedestrian areas.
- Seating/resting areas should have seats with backrests and arms on both sides.

MATERIALS AND PRODUCTS

- New built in street furniture should use a colour palette and texture palette that reflects the materials of existing buildings, including yellow stock brick and purple engineering brick colourings and in situ concrete finishes.
- All freestanding street furniture must be powder coated black.
- Cycle stands: Black Sheffield stands
- Litter bins: Black cast iron (Broxap Pierhead Tall or similar)
- Bollards: Black cast iron (Broxap Capital or similar)
- Off the shelf benches: (Woodhouse Esfet Bancalosa or similar)
- Drinking fountains: (Santa & Cole Atlantida or similar)
- In ground/ pop up power units: (Pop Up Power: IGU - In Ground Units or similar)

LIGHTING

GENERAL

New lighting should be consistent and lux levels appropriate to use, including avoiding pooling of light and shade to aid people with visual impairment.

New lighting should reduce risks of night-time accidents; assist in the protection of property; discourage crime and vandalism; make residents and street users feel secure and enhance the appearance of the area after dark.

New lighting should signify the hierarchy of streets and be consistent.

Coordination between the highways authority and adjacent landowners should be undertaken ensure that similar standards of lighting are provided for the adopted highway and un-adopted areas.

New lighting should be designed to minimise clutter. Luminaires should be mounted on passages, and column-mounted luminaires must be located at the back of footways on historic streets.

In ground light fittings must not be used, to minimise physical obstructions and due to maintenance issues.

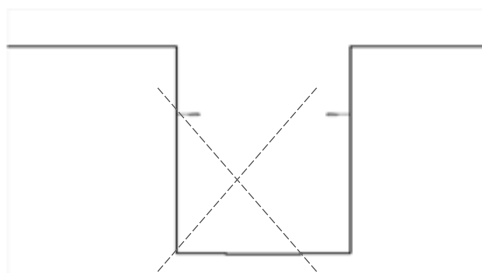
Lamp columns should serve dual purpose where possible for example on historic streets should be combined with any proposed signage to minimise clutter.

Lighting should provide amenity and , e.g. lighting combined with seating.

New lighting should be designed to minimise energy use and whole life costs and minimise light pollution, particularly near sensitive wildlife habitats.

BUILDING MOUNTED LIGHTING

All passages and public spaces should have building mounted, apart from historic streets. Where this is not possible, lighting must be placed to the back of the footway to keep pedestrian routes as clear as possible.



Building mounted lighting in Passages and Yards

Building- mounted lighting must be mounted at heights and with cones which are tested to not conflict with the residential or commercial occupation of buildings with rooms facing the street.

Spacing of building- mounted lighting units must be kept to an appropriate minimum to achieve the required lux levels for individual areas.

MATERIALS AND PRODUCTS

Finishes of lamp columns and building- mounted lighting must be black.

Building mounted lighting (Philips Iridium or similar)

Lamp columns (Philips Iridium or similar)



Building Mounted Luminaire

Colour: Black

Suggested Supplier: Philips

Product name: Iridium ZRS250

Other: Wall mounted lighting (non adoptable areas)



Column and wall mounted lighting

Colour: Black

Suggested Supplier: Philips

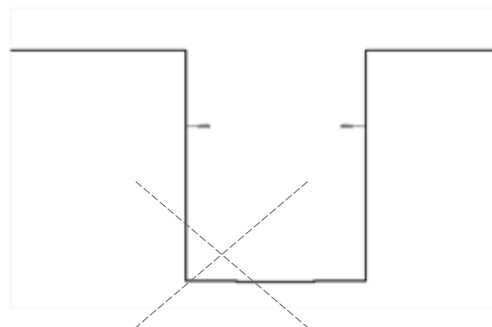
Product name: Iridium

Other: Column and wall mounted lighting (adoptable areas)

LIGHTING FROM THE FOOTWAY

Free standing lamp columns must be located to the rear of the footway – allowing for the greatest clear width of footway possible.

Where possible street signage and light columns should be combined to avoid cluttering of footways.



Back of footway lighting for Streets

STEPS, RAMPS AND RETAINING STRUCTURES

GENERAL

New steps, ramps and retaining structures must incorporate gradients as shallow as possible and in compliance with the LLDC Inclusive Design Standards.

New steps, ramps and retaining structures must be designed to be simple and robust and reduce the risk of vandalism and maintenance costs.

New steps, ramps and retaining structures must be of a consistent approach to finish and colour, that is appropriate to context as per the material and product codes.

New steps, ramps and retaining structures should be kept to an absolute minimum to avoid negative impacts on the street such as creation of physical barriers and visual clutter street furniture.

New steps, ramps and retaining structures should be designed to also serve as places to meet, sit and be used as spaces of informal play where level changes occur (notably at the canal edge).

New steps, ramps and retaining structures should be integrated wherever possible and opportunities for dual functions such as steps doubling as seating elements or opportunities for play should be maximised.

New steps, ramps and retaining structures should be designed to encourage use for seating, but care should be taken that this does not compromise drainage and other technical considerations.

ACCESS TO BUILDINGS:

New buildings must have level landings into entrances (1:60 or less steep).

The approach to the principal entrances of new buildings must be step-free.

All principal entrances should be clearly identifiable within the façade of the individual buildings and have a positive relationship with the public realm.

Entrances should support natural surveillance and wayfinding.

Principal entrances should have suitable overhead and side covering to protect users from elements such as wind, rain and glare from the sun.

New buildings should create active frontages on to the public realm to maximise public recognition and use.

RAMPS

New ramps must have a maximum gradient of 1:21, with level landings for every 500mm change in elevation.

New ramps with a gradient of 1:30 or less must have resting points if provided off circulation routes.

Gradients should be integrated into the broader levels of the site, where possible.

MATERIALS AND PRODUCTS

Where a localised change of level is required to form connections combined with steps, materials should match.

Cast in-situ concrete should be used in preference to timber.

Typical stairs should be constructed in a cast in-situ concrete with an exposed aggregate finish. In this instance, the finish must be continuous to all surfaces including vertical faces of retaining walls and stair treads.

If handrails are required these should be constructed in prefabricated steel with a simply detailed rolled edge and minimal vertical down stand supports giving a lightweight and elegant quality to appearance.



Changes in level, steps and ramps should be designed to create informal seating and play

WAYFINDING AND SIGNAGE

GENERAL

Wayfinding must be taken into account at all stages of the design process.

New signage must ensure sight lines, lighting, visual contrast and legibility are appropriate.

Building planning layout must consider the logical layout of facilities and the need to support ease of wayfinding.

Signage should be simple and consistent.

Applicants should work with adjacent plots to develop a comprehensive approach to signage to ensure consistency and avoid duplication.

Opportunities for reinstating signage on heritage buildings, uncovering historic murals and opportunities for interpreting of the area's heritage should be maximised.

New yard signage should be included where relevant and, in particular draw attention to public facilities and avoid the need for multiple signs.

Legible London signage should be installed on key spaces and on key pedestrian and cycle routes.

Signage should be dual purpose and consolidated where possible, for example signage mounted on lampposts or combined.

The location and design of signs and signposts should be planned to permit effective maintenance (including access for cleaning equipment) and to minimise clutter.

Street layouts, geometries and networks should aim to make the environment self-explanatory to all users.



Legible London Signage



Signage integrated with other public realm elements, Bankside



Simple and legible signage identifying public uses, Yard Theatre



Wayfinding and signage giving businesses a presence on the street, (Blackhorse lane - We Made That)

PLAY IN THE PUBLIC REALM

GENERAL

- Applicants must work together so that play provision is considered as an integral part of a holistic 'playable' public realm strategy.
- Play in the public realm must be designed to avoid conflict with traffic and dogs.
- Play provision for 0-5 year olds must be provided within development plots
- Play provision within development plots (i.e. in traffic free spaces) must maximize opportunities for physical play, swinging, climbing and water play and be designed to maximize passive surveillance.
- Canal- side play provision should not be fenced off, and must be accessible and cater for all abilities.
- Play spaces must also cater for adults, for example providing adequate seating.
- Public spaces and streets should be considered together to create a journey or network of playable spaces, allowing children to roam, play and choose a number of routes.
- Street furniture should be designed to encourage appropriation for play, for example level changes, stepping stones and benches that double as playable elements.
- Applicants should seek opportunities for playable elements and equipment to be locally made and respond to the distinctive character of the area and designed into the public realm, rather than introduce a new visual language.
- Play equipment should be multi- use and designed to promote imaginative play, rather than sole reliance on 'off the shelf' play equipment.
- Natural play should be encouraged in the public realm and within development plots through sensory soft landscaping and tree planting.

For further guidance, see LLDC Youth Play Study 2016



View to canal edge showing playable public realm, note feature benches and cluster of playable elements around new tree planting



Example typical playable canal facing courtyard space with topographic play / edges and landscape elements



Examples of Playable public realm, left: Brinkworth Way, Hackney Wick - Muf Architecture Art. Right: The Goods Line, Sydney - Aspect Studios

HARD LANDSCAPE MATERIALS

GENERAL

- New proposals must adhere to correct material palette for each Public Realm Typology.
- Proposals must complete a hard landscape materials site audit before commencing detailed design, in order to identify opportunities to recycle and reuse on site materials and uncover historic materials and features. For example casting existing cobbles into concrete to make a flush surface or using demolition material as aggregate in in- situ concrete.
- Where there is risk of vehicle overrun, appropriate build up must be specified.
- Hard landscape materials must be installed with any necessary joints and flush to prevent small wheels, walking sticks and canes being trapped.
- Hard landscape materials should have a cross fall no greater than 1:50.
- Hard landscape materials with busy patterned surfaces such as stripes that cause confusion or are disorientation in particular to people with visual impairments should be avoided.
- Hard landscape materials should be integrated into surrounding area that includes tactile surfaces at controlled crossing points.
- There should be a strong focus on using locally made elements to draw on the talents of businesses across Hackney Wick and Fish Island, London and the UK.
- Hard landscape materials should have a surface which is even and stable.
- Changes of material should be used as opportunities for demarcating space, for example extending a tree pit surface to create a space to sit around a tree.
- Proposals should promote sustainable design including using materials that have a low carbon footprint.
- Proposals should include recycled and reclaimed materials where possible, it is recommended that a minimum of 20% concrete should be recycled/ secondary content.
- Proposals should target 40% of materials used having a rating of B or higher and must ensure that no materials with a rating lower than D are used.
- In the case of timber, Proposals must target 100% use of responsibly sourced Forest Stewardship Council (FSC) or Programme for the endorsement of forestry (PEFC) certified timber products.

HARD LANDSCAPE PALETTE



Product: Standard Grey Flag ASP Pavers

Colour: Natural / Standard Concrete Finish

Format: 450 x 600mm

Other:

- Sand bedded footways surface ideal for utility access / ease of reinstatement

- All covers to be cast iron and recessed type

- Existing cast iron to be retained



Product: Tarmac



Colour: Silver Grey Granite (kerb only)

Material: Granite

Finish: Flamed Finish

Format: 915 x 255 x 125mm

Suggested Supplier: Charcon

Other:

- Drainage - either gully made out of granite or cast iron

- Square edges



Product: Super Cedec

Colour: Gold

Aggregate size / spec 0-6mm

Suggested Supplier: CED

Other:

- Permeable

Self binding

- Seeded with wild flowers and allowed to colonise naturally



Granite setts to central zone

Colour: see opposite

Material: Granite

Finish: Flamed surface

Format: 150-325 Random(L) x 90(W) x 150(H)mm

Suggested Supplier: CED

Other:

- Cropped sides
- Stretcher bond with 'zip' edge



Granite setts to central zone

Colours:

Bluish Grey	5%
Blue/Grey	37.5%
Red (Dark)	10%
Green	37.5%
Black	10%



Granite setts to edge of space

Colour: Dark/Blue grey

Material: Granite

Finish: Flamed surface

Format: 150-325 Random(L) x 90(W) x 150(H)mm

Suggested Supplier: CED

Other:

- Cropped sides
- Stretcher bond with 'zip' edge



Canal Edge

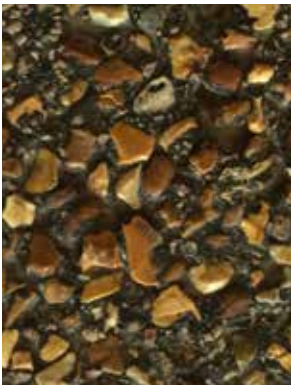
Primary Material: Concrete

The canal edge may also be formed by extension, replacement or repair of existing materials such as

-steel sheet piling

- Granite

- Other existing concrete



Product: Ground Asphalt

Finish: Ground Finish



Poured Concrete



Cast Concrete bench and low wall



London Stock Brick

Reclaimed brick used for hardscape flooring

PRIMARY STREETS

Hackney Wick and Fish Island is defined by a street pattern that evolved out of the pre-urban, rural, winding lanes such as Wallis Road and White Post Lane. The objective for primary streets is to form strategic north to south connections from Hackney Wick to Fish Island, and east to west connections from Victoria Park and Hackney through to Queen Elizabeth Olympic Park and Stratford. With the introduction of the new station entrance and the North South Route the historic streets form the primary network of pedestrian cycle and vehicle access for the neighbourhood centre and new development on these streets should reflect this.

An objective of new development is to reinforce the character of primary streets with new development of robust, simple and restrained built form and character with well-defined frontages at the back of the pavement and appropriate massing which is sensitive to the setting of heritage assets. New development should also reinforce primary streets as pedestrian and cycle routes, by providing a focus for new retail and community uses, active frontages, new street trees, inset parking and more generous footways- which are all made possible due their width (approx. 12m).

MATERIALS AND PRODUCTS

Paving: Grey Flag ASP pavers, natural/ standard concrete finish (450 x 600mm)

Kerb: Silver grey granite, granite, fine picked finish, (915x150x125mm)

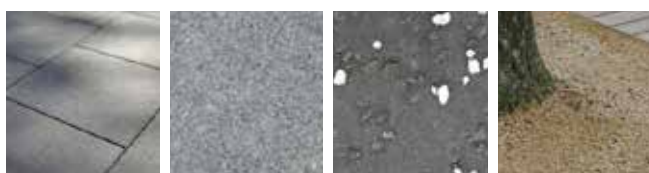
Carriageway surface: Hot rolled asphalt

Tactile paving: Blister tactile paving, texture natural

Vehicle crossovers: Concrete block pavers, (100 x 200mm)

Tree pit edging: Silver grey granite, granite, fine picked finish, (915x150x125mm)

Tree surround: To be of a permeable, flexible, self-bound gravel in grey (Cedec or similar) or resin bound granite (grey)



Primary Streets Materials Palette

PUBLIC REALM

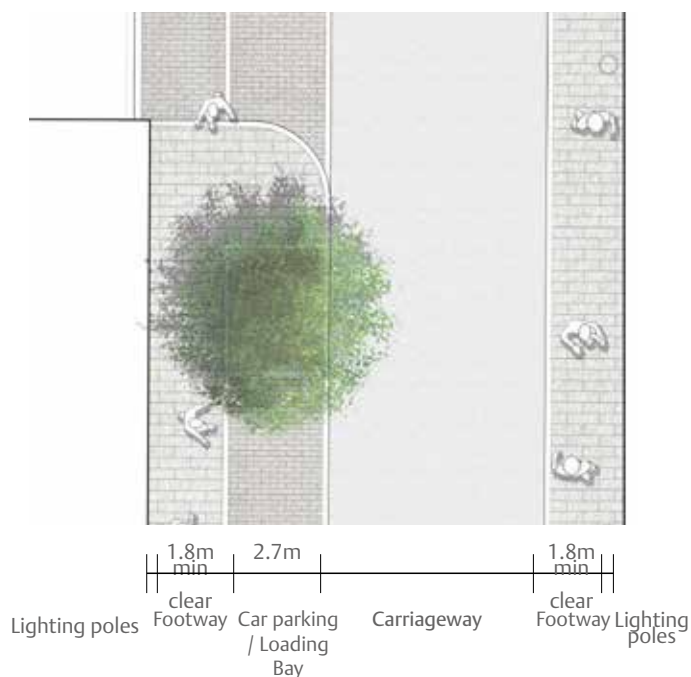
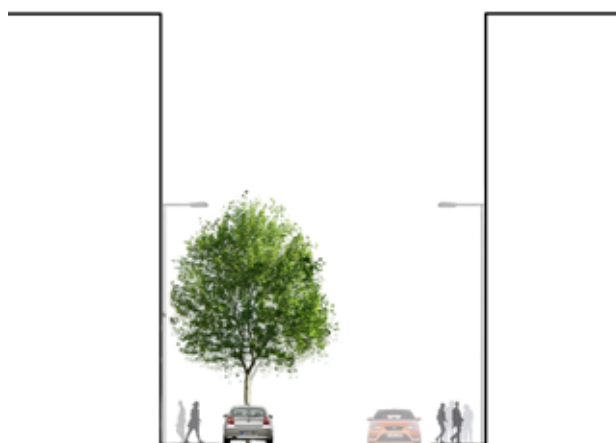
Proposals must reinforce the character of historic streets as traditional streets providing primary access for vehicles, pedestrians and cyclists, with a clear distinction between the footway and carriageway defined by a 125mm facing kerb.

Proposals must include provision for inset on street parking and loading bays, including visitor blue badge parking bays with easy access to the station and canal edge.

New street trees must be provided.

Street lighting must be positioned at back of pavement.

Proposals must follow recommendations for materials as set out in the site-wide material palettes in Section 2.



Illustrative plan and section of typical Historic Street Condition

BUILT FORM AND CHARACTER

GENERAL

Building heights should step down in response to the setting of heritage assets, and step up in Tall Building locations. A minimum of 2 storeys should be maintained to ensure good definition of the street.

New development must have well-defined frontage with the building line at the back of the footway. Vehicle forecourts are not acceptable.

ELEVATIONS

Elevations must be simple and uncluttered, using a reduced number of elements so not to become unnecessarily complex.

Projecting balconies must be avoided and use of canopies, projecting signage, downpipes or any other ancillary element protruding outside of the building line should be kept to an absolute minimum.

Elevations should have a regular pattern of openings and must respond to the material, scale and proportions of retained heritage assets.

New development must have active frontages on the ground floor (approx. 70% of primary street frontage) and be a focus for retail and community uses.

Elevations should create a subtle distinction between ground and upper level uses.

Blank frontages should be avoided, for example by locating refuse stores and plant rooms on secondary elevations (not on primary streets).

Elevations should have a predominantly consistent and simple roofline, which does not compete with or detract from retained heritage assets.

Rooftop amenity must be integrated into the mass of the building to avoid adding unnecessary complexity to the roofline.



Illustrative elevations describing the level of variety that can be achieved within street facing elevations



SECONDARY STREETS AND PASSAGES

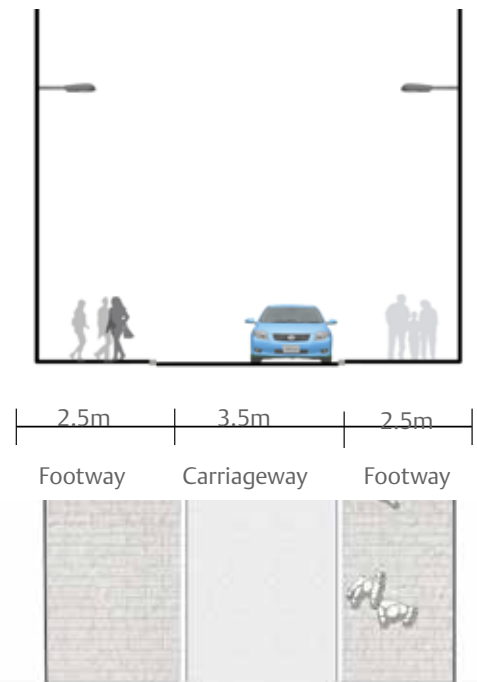
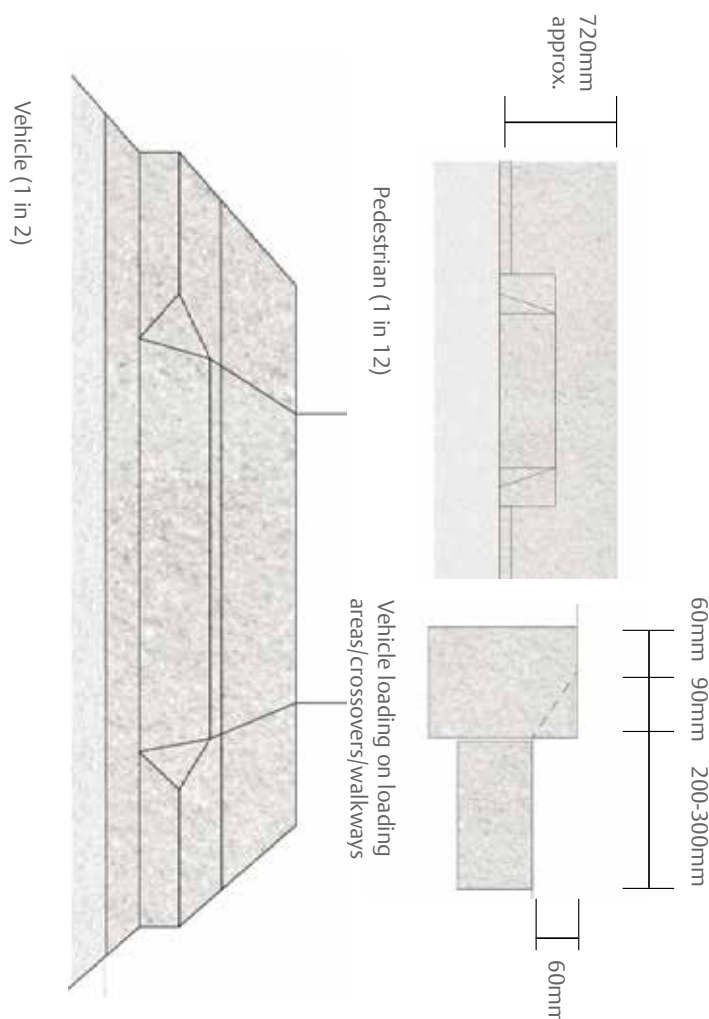
The key objective for passages is to create new routes across development plots, which establish a secondary network of connections to create a more permeable and walkable public realm. They also recall the historic grain of development of passages and smaller yards. In some locations the passages also better reveal heritage assets to the public realm. They also provide opportunity to create new consolidated utility routes between sites and some limited vehicle movement to service buildings. Due to their width (approx. 10m) new development on passages must be designed to ensure adequate levels of daylight and sunlight are achieved, as well as adequate privacy between facing homes. A key design objective of the guidance is to create a more informal and varied approach to facade composition and stepped or articulated roof profiles in contrast to the more restrained frontages of historic streets, to create a variety of character in the area and reinforce the hierarchy of existing streets. Most development on passages will comprise of workspace on the ground floor and/ or first floors, with residential above.

PUBLIC REALM

In order to meet the public realm design objectives, the Applicant must demonstrate that proposals adhere to the site-wide public realm codes set out in Section 2, as well as the following that are specific to passages:

- Proposals must reinforce character of passages as traditional streets providing access for vehicles, pedestrians and cyclists, with a clear distinction between the footway and carriageway defined by a 60mm facing kerb.
- Proposals should include inset parking bays and loading bays as set out in the parameter plans.
- New street trees must be provided where possible- although this is unlikely due to street width.
- Street lighting should be building mounted
- Proposals must follow recommendations for materials set out in the site wide material palettes in Section 2

Illustrative plans and diagrams illustrating approach to vehicle crossovers

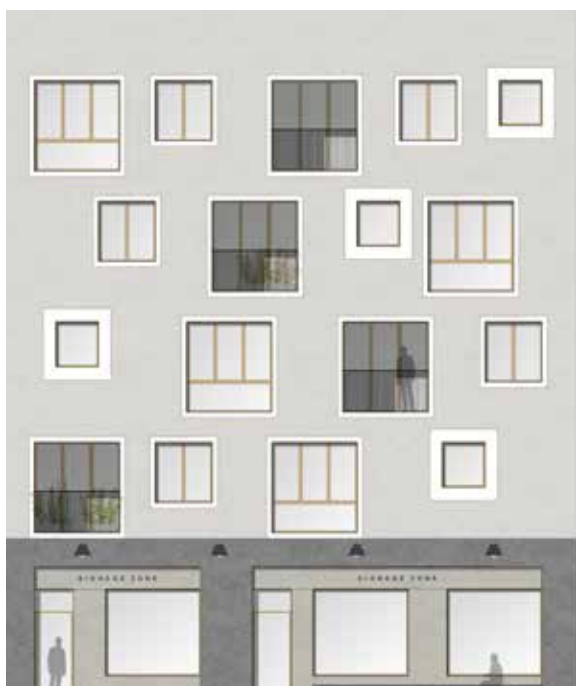


Illustrative plan and section of typical Passage Condition

BUILT FORM AND CHARACTER

GENERAL

- Building heights should step down in response to the setting of heritage assets where appropriate and step up in Tall Building locations. A minimum of 2 storeys should be maintained at ensure good definition of the street.
- North facing elevations on the east-west passages should include steps in height to ensure good daylight and sunlight to the passages.
- South facing elevations should include steps in height to ensure good daylight and sunlight to courtyards and internal block elevations.
- To mitigate privacy and overlooking issues, the majority of units on passages should be dual aspect, and all balconies onto passages should be recessed into the built form.
- New development must have well- defined frontage with building line at the back of the footway. Forecourts are not acceptable.
- Building corners should be chamfered at ground and first floor where necessary to maintain minimum pavement widths for wheelchairs users, etc., and enable vehicle turning for refuse vehicles and fire tenders.



ELEVATIONS

- Elevations must be simple and uncluttered.
- Elevations should have a more informal and varied facade composition in contrast to the more composed frontages of street facing elevations.
- Roof lines in passages can also be more varied with stepped or articulated roof profiles where rooftop amenity spaces are integrated.
- Projecting balconies should be avoided and use of canopies, signs, downpipes or any other ancillary element protruding outside of the building line kept to an absolute minimum.
- Elevations should create a subtle distinction between ground floor and upper level uses.



Illustrative elevations describing the level of variety that can be achieved within passage facing elevations

CANAL EDGE

The River Lee Navigation and Hertford Union Canal are very important to the character and industrial heritage of Hackney Wick. Proposed development should create better access to the canal edge, encourage use of the waterways, provide a welcoming environment for families and children as well as create an active frontage with workspace and retail uses on the lower floors. The canal edge is significantly higher than the rest of the site and changes of level must be made accessible and designed to support informal play, provide places to sit and new landscaping along the water's edge. Buildings have historically been set back from the canal edge to create different sized working wharves for different uses, each individually owned with their own material character in contrast to the continuous tow path on the east side of the canal. To reflect this varied character of the canal edge, it is important that new development use hard landscaping materials that are subtly differentiated and reveal historic surface materials. Moreover, variety should be achieved through different building heights, distances between buildings and set backs from the canal edge.

MATERIALS AND PRODUCTS

Paving: Sand blasted or ground hot rolled asphalt or in situ concrete with exposed aggregate finish

Kerb: Silver grey granite, granite, flamed finish, (915x255x125mm)

Tactile paving: Blister tactile paving, texture natural

Vehicle crossovers: Concrete block pavers, (100 x 200mm)

Tree pit edging: Silver grey granite, granite, fine picked finish, (915x150x125mm)

Tree surround: To be of a permeable, flexible, self-bound gravel (gold)

Paving: Sand blasted or ground hot rolled asphalt or in situ concrete with exposed aggregate finish

Kerb: Silver grey granite, granite, flamed finish, (915x255x125mm)

Carriageway surface: Hot rolled asphalt

Vehicle crossovers: Concrete block pavers, (100 x 200mm)

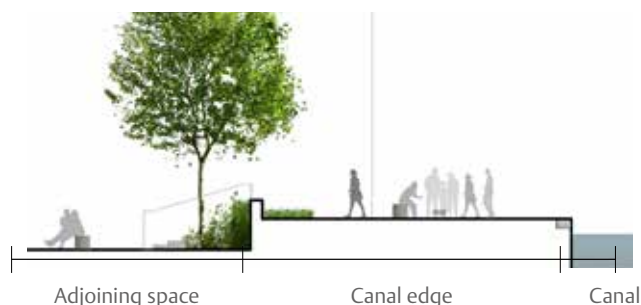


Canal Edge Materials Palette

PUBLIC REALM

In order to meet the public realm design objectives, the Applicant must demonstrate that proposals adhere to the site-wide public realm codes set out in Section 2, as well as the following that are specific to the canal edge:

- Proposals must provide clear zone of 1.8m for pedestrian movement.
- Proposals must provide accessible ramps to the canal edge. New ramps to the canal edge should be integrated and provide opportunities for seating and informal play and must not create physical barriers or visual clutter.
- Access between canal edge plots should be encouraged but not as a continuous towpath as proposals should reflect the historic character of the canal edge as series of private terraces or wharves.
- Proposals should follow recommendations for materials set out in the site wide material palettes in Section 2, but some differentiation between plots is encouraged and historic materials and surfaces should be revealed.
- Seating should be inbuilt. In order to promote the use of the canal edge by families and children, inbuilt seating and hard landscaping should be designed to double as playable elements.
- Opportunities for new planting and ecological enhancement must be maximised, for example reed beds and in-ground planters. Raised planters should be avoided, to minimise clutter and physical obstructions.
- Lighting on the canal edge must be designed to minimise ecological impacts on the waterways.



Typical plan & section

BUILT FORM AND CHARACTER

GENERAL

- Building heights should step down in response to the setting of heritage assets where appropriate.
- New development should integrate existing changes of levels to create generous ground floor spaces this should be considered in the design from an early stage.
- Internal finish floor level must be set at the level of the adjoining canal edge
- The building line of new development on the canal edge must be parallel to and set back from the river wall, but blocks should have different offset distances
- Buildings should ideally be set back more than 6m from the canal edge.
- Buildings should be designed so as to encourage ground floor activities to spill out into the canalside spaces.



ELEVATIONS

- Elevations must be simple and uncluttered.
- Elevations should have a composed but varied facade composition. For buildings that can be viewed from a distance the proposed scale and modulation of façade elements should take this into account for example creating larger apertures.
- Buildings must have a parapet and roofline should be designed to hide any lift overruns or plant from view.
- Canal facing frontages must be broken by a full height cut to the canal side courtyard or step in the elevation to create a finer grain and elegant proportions.
- In order to maximize access and views from residential courts to the canal blocks should include large-scale ground floor openings with minimum dimensions of 8 meters wide and two storeys high.
- External balconies should be restricted to the second floor and above, in order to maintain clear and uncluttered canal edge and maximise views.
- New development should have simple and defined roof line with clear and deliberate steps in height to create a simple silhouette along the canal
- A two stories high datum should be articulated in façade design to respond to the height of retained heritage assets



Illustrative elevations describing the level of variety that can be achieved within canal facing elevations

YARDS AND COURTS

In order to encourage use and create a strong sense of place, private realm within development blocks should have a very clear and unambiguous character that relates to its singular function and use, i.e. as a working yard or a residential courtyard.

Working yards

Workspace in Hackney Wick and Fish Island has traditionally been arranged around working yards and the remaining yards still support the culture of creative making in the area, however frequently yards are dominated by informal parking and poor access. New working yards are envisaged to support a new generation of productive and creative uses in smaller scale work spaces and provide short term parking, servicing and loading bays for smaller vehicles. Working yards should be simple, flexible and open spaces that encourage use, with hardwearing surface treatment and without built in furniture or raised planters and trees. Frontages onto working yards should be predominantly for employment uses. Historic yards must maintain a public right of way through them in order to maximise public access.

Residential courts

Residential courtyards are fundamental in providing access to daylight and air for surrounding dwellings and will provide high quality amenity space including doorstep play and a rich planted landscape in contrast to the traditional street character of the neighbourhood centre public realm. In general, residential courts are raised to podium level with undercroft parking or large-scale workspaces beneath, with the exception of canal-side courts, which are at grade in order to maximise access and views through to the canal. The canal side courts may have some public access through and views into them, therefore canalside blocks must also provide rooftop play provision and residential amenity with adequate sunlight and privacy for those living in the block.



View of a residential raised courtyard (Illustrative scheme)

MATERIALS AND PRODUCTS

NORTH SOUTH ROUTE & HISTORIC YARDS

Paving 'central zone': Granite, flamed finish, 150-325mm random (L) x90 (W) x 150 (H), cropped sides laid to stretcher bond, 5 colours laid at random (Bluish grey, Blue/ grey, Red (dark), Green, Black)

Paving 'pedestrian comfort zone': Granite, flamed finish, 150-325 random (L) x90 (W) x 150 (H), cropped sides laid to stretcher bond with 'zip' edge, Colour: Dark Blue/ grey

Kerb: Silver grey granite, granite, flamed finish, (915x255x125mm)

Tactile paving: Blister tactile paving, texture natural

Vehicle crossovers: Concrete block pavers, (100 x 200mm)

Tree pit edging: Silver grey granite, granite, fine picked finish, (915x150x125mm)

Tree surround: To be of a permeable, flexible, self-bound gravel (Cedec or similar) or resin bound granite (granite grey)

WORKING YARDS

Paving: Sand blasted or ground hot rolled asphalt or in situ concrete with exposed aggregate finish

Kerb: Silver grey granite, granite, flamed finish, (915x255x125mm)

Carriageway surface: Hot rolled asphalt

Vehicle crossovers: Concrete block pavers, (100 x 200mm)

RESIDENTIAL COURTS

Paving: London stock brick or clay pavers

Kerb: Silver grey granite, granite, flamed finish, (915x255x125mm)

Vehicle crossovers: Concrete block pavers, (100 x 200mm)

Tree pit edging: Silver grey granite, granite, fine picked finish, (915x150x125mm)

Tree surround: To be of a permeable, flexible, self-bound gravel (Cedec or similar) or resin bound granite (grey)



Working Yard Materials Palette

BUILT FORM AND CHARACTER

GENERAL

- Massing must include adequate steps in height to ensure good sunlight and daylight to inner block elevations and to adjoining blocks.
- New development should include full height breaks in buildings where residential courtyards link to passages to provide daylight into public realm.
- Rooflines should be designed to encourage occupation of roof top amenity spaces
- Rooflines must step down into residential yards to improve sunlight to shared amenity spaces
- In working yards internal finished floor levels must be at the level of the working yards

ELEVATIONS

- Elevations must be simple and uncluttered.
- Similar to passages, internal elevations onto residential courts should have a more informal and varied facade composition in contrast to the more composed frontages of street facing elevations.
- Lighter coloured materials should be adopted within courtyard areas to improve the quality of daylight



RESIDENTIAL COURTYARD ELEVATIONS

- Elevation design must ensure adequate privacy for residential units, for example the use of gallery access is encouraged to manage privacy and create a buffer zone.
- Private amenity spaces and external circulation should be designed to encourage occupation and use by residents
- Proposals should follow recommendations for materials set out in the site wide material requirements in Section 2, but use of softer and more tactile materials should be used to bring a domestic quality to residential courts
- Planting on blank walls should be used to reduce areas of blank frontages to courts

WORKING YARD ELEVATIONS

- In working yards special consideration must be given to minimise conflict between residential and employment uses, for example using gallery access to create a buffer zone, or stepping residential units back at upper floors back from the working yard.



Illustrative elevations describing the level of variety that can be achieved within yard facing elevations

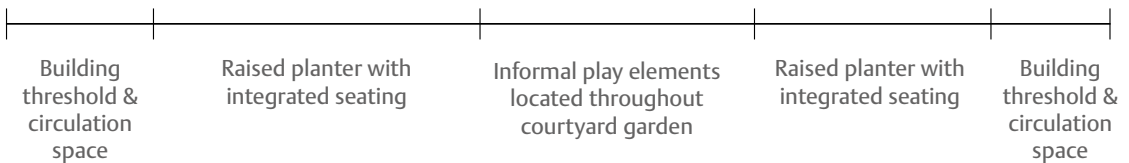
RESIDENTIAL COURTS

PUBLIC REALM

- Residential courtyards must provide 0-5 play space.
- Residential courts that are raised to podium level must be accessible via a new ramp or internal lift.
- Podium construction should be designed to allow for mature trees
- Hard landscape materials should follow recommendations for materials set out in the site wide material palettes in Section 2, but should be in keeping with the colour, tone and materiality of architectural proposals to ensure sense of consistency and quality.
- Proposals for raised beds and planters should provide opportunities for informal play and seating

CANALSIDE RESIDENTIAL COURTS

- Canal side blocks must have residential entrances off the street or within courts, not on the canal edge.
- Canal side blocks must have rooftop play and residential amenity spaces in addition to residential court provided at grade
- Hard landscape materials should follow recommendations for materials set out in the site wide material palettes in Section 2, and should allow the canal edge surface to extend back into the depth of the block to ensure material continuity.
- Canalside residential courts should be visible from the public realm and should preferably have some degree of public access, but should not contain activities that would disrupt residents.

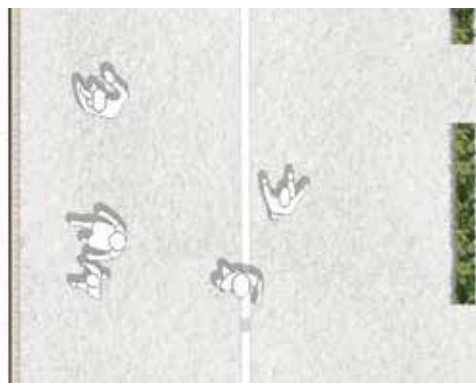


Illustrative plan and section of residential court

WORKING YARDS

PUBLIC REALM

- Clear turning and manouvering space must be provided to allow vehicle access
- Proposals should follow recommendations for materials set out in the site wide material pal-ettes in Section 2, but some differentiation between plots is encouraged and historic materials and surfaces should be revealed.
- Proposals must not include tree planting or raised planters
- The hard landscaping should be laid to a fall to provide good drainage, this should be to a central channel or a channel along building line.
- The planting of climbing/trailing plants on south and west facing walls should be used, but must be avoided on historic elevations of retained assets.
- Seating should be removable and not built in.
- Lighting should be building mounted.



Illustrative plan and section of working yard

SOFT LANDSCAPING AND STREET TREES

GENERAL

Planting has the potential to ‘add value’ to a development whether through character, local provenance, the encouragement of biodiversity, or other functional uses.

In each instance, planting must be shown to be adequately specified, fit for purpose and robust to ensure longevity. It is vital that long term management and maintenance are considered in parallel with design. Where appropriate expert advice, ecological or horticultural, should be demonstrated as part of the process: for example, where green roofs, green walls or SUDs are proposed.

- Planting pockets must be a minimum of 700mm deep to ensure optimal growing conditions for low level planting. This does not include tree pits.
- Planters must be built-in and flush with footway level, unless located where the footway widens and opens into key public spaces.
- Promoting green infrastructure and native biodiversity must be a key consideration in selecting plant species for the scheme.
- Non-native species proposed must be well adapted to the present and future London climate and capable of supporting native biodiversity populations as well as native species.
- Plant species must be carefully selected to encourage residents and visitors to engage with the environment; by utilising plants that are of an attractive form, will flower, provide colour, attract wildlife and provide change through the seasons.
- All proposals must describe a management and maintenance program for all landscape works within proposed development on a plot by plot basis.
- Detailed design of tree pit should consider current best practice, and incorporate adequate soil volumes, good quality topsoil, protection to surrounding pavings and adjacent services, and

drainage provision if necessary.

- Trial pits must be undertaken to establish the quality of the topsoil, possible drainage problems, and the presence of any underground services.
- Where there is concern that adjacent services, such as gas and water pipes and electricity cables, that might be affected by growing root systems, an impenetrable barrier should be installed which will prevent damage by future root growth.'
- The following issues must be given consideration when making a choice of trees for planting adjacent to the public highway
 - ultimate mature height - small (to approximately 10m), medium (to approximately 15m), large (greater than 15m)
 - Ultimate shape and spread – in general, when planting adjacent to highways, particularly where high sided vehicles regularly pass, it is preferable to select a variety with relatively upright growth that will not require regular pruning or be damaged by the passage of vehicles. Away from the immediate kerb line a broader crown could be considered.
 - Site conditions – as long as the soil is free draining and near-neutral pH then most commonly specified street trees will grow well
- Tree pit surfacing should consist of flush porous aggregate as per the hard landscape materials set out on the previous page.
- In key/unique spaces, trees should be strategically located as single, specimen trees or clusters of single species trees to create strong focal points of interest, colour and structure within the public realm.
- The following is a non-exhaustive list of trees suitable for planting.

PRIMARY STREETS:

Medium sized street trees such as:

- *Tilia cordata* 'Greenspire', semi-mature. Mature height 12-17m
- *Quercus palustris*, semi-mature. Mature height 18-20m

Large sized street trees such as:

- *Platanus hispanica*, semi-mature. Mature height 20m+
- *Acer pseudoplatanus* 'atropurpureum' - Purple Sycamore - Mature height 20m+



Tilia cordata 'Greenspire'
- Small leaved lime



Platanus hispanica -
London Plane

SECONDARY STREETS AND PASSAGES:

Medium sized street trees such as:

- *Liquidambar styraciflua*, semi-mature. Mature height 12-17m
- *Acer campestre*, semi-mature. Mature height 15-25m

Large sized street trees such as:

- *Tilia cordata*, semi-mature. Mature height 20m+
- *Alnus cordata*, semi-mature. Mature height 20m+



Quercus palustris - Pin
Oak



Liquidambar styraciflua -
Sweetgum

KEY SPACES:

Suitable examples:

Liriodendron tulipifera
Carpinus betulus
Fagus sylvatica
Acer platanoides
Pinus sylvestris/nigra
Catalpa
Tilia x europaea



Liriodendron tulipifera - Tulip Tree

RESIDENTIAL STREETS:

Suitable examples:

Pear
Sorbus aria
Malus
Magnolia
Ligustrum japonicum
Crataegus



Magnolia

TREES IN PRIVATE REALM:

- Proposals for trees in residential courts should include small, ornamental trees which provide visual interest throughout the year, with changing foliage colour and interesting form and bark or a mixture of single and multi-stemmed trees to add variety and interest

- Suitable examples:

Amelanchier lamarckii (multi-stem) - Mature height 3-7m

Betula albosinensis 'Fascination' - Mature height 7-12m

Betula utilis var. *jacquemontii* - Mature height 12m+

Cercidiphyllum japonicum (multi-stem)- Mature height 12m+



Amelanchier lamarckii - June Berry



Cercidiphyllum japonicum - Katsura Tree

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